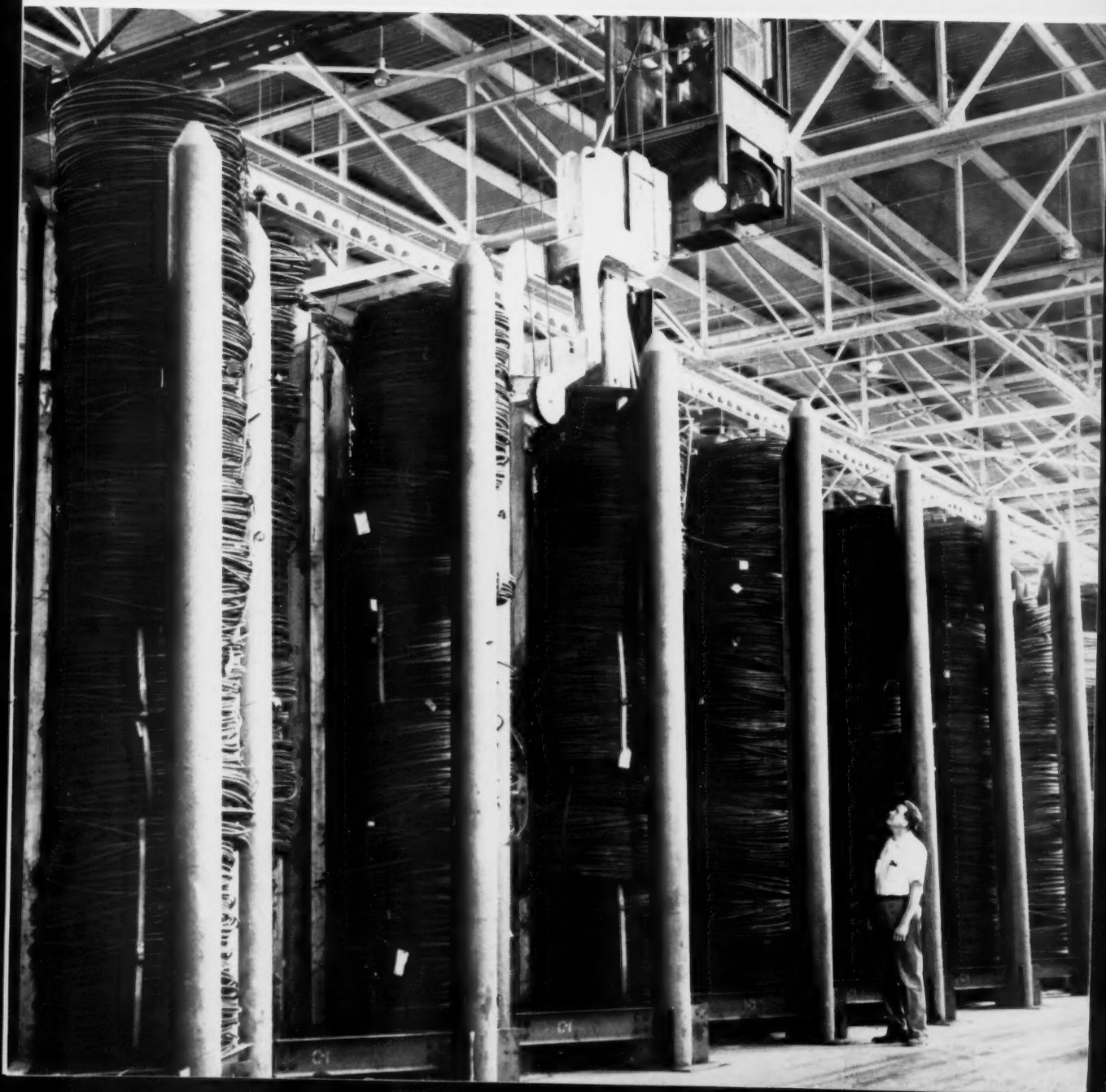


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Mercury "Tug" Electric Tractor is ideally suited to installations requiring quiet, fume-free operations.



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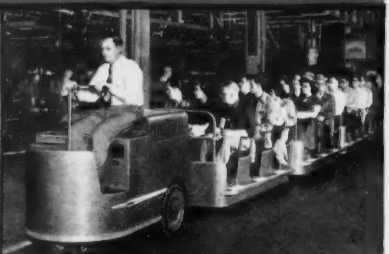
Mercury "Banty" Gas Tractor powers the "Trackless Train" system. Cuts time, distance and cost.



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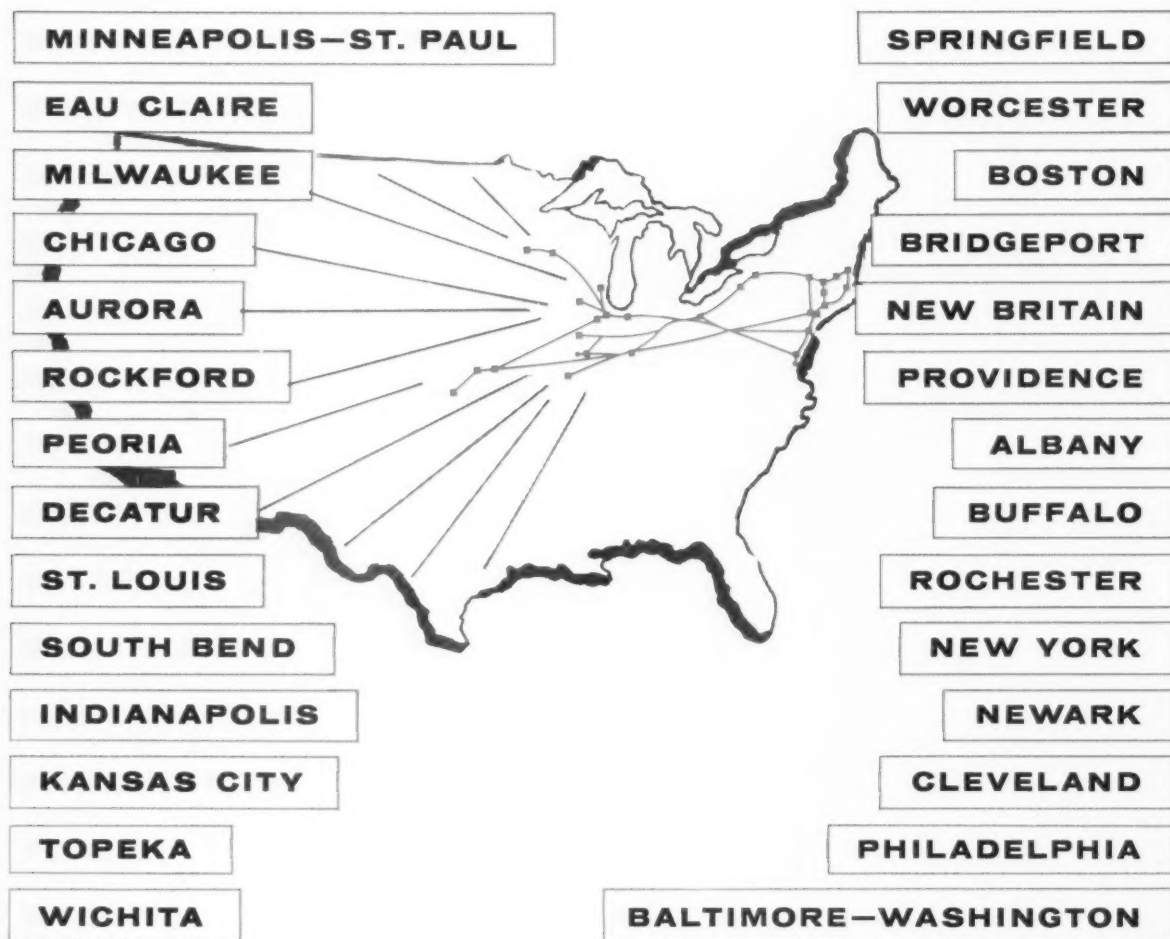


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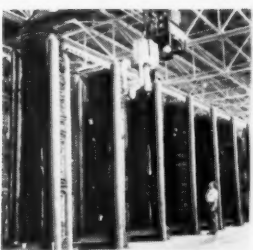
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THE COVER

Overhead crane with "alligator
jaws" stores coils of wire in
vertical cribs. Turn to Page 36

DISTRIBUTION AGE

Vol. 56, No. 5

ESTABLISHED 1901

May 1957

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ATLANTA PAPER CO.—"We put the *Clarklift* through our toughest performance tests," reports Frank Marshall, Director of Traffic. "It outdid anything else, actually set new standards in our evaluation of fork trucks. As a result, we're so sure the *Clarklift* is the best truck of its kind on the market, we ordered 12 of them."

HERE'S WHAT ATLANTA THINKS OF THE NEW CLARKLIFT

Greater Atlanta has a population of 885,000, yet over 33,000 U.S. companies have representation there. "Nearly everyone has an office, a plant or a branch here . . .," including the Federal Government which maintains 76 different agencies in Atlanta. This city has been called the hub of the Southeast, the transportation and communications center of the South, a center for modern industry.

Landscaped, brightly colored factories dominate the industrial areas. More than a quarter of the manufacturing plants are new. This city has awakened to its

role as an industrial giant. It's this newness, this industrial awareness that reflects best the innovation that is *Clarklift*.

Clarklift is a fork lift truck. Four years ago it was a model designed for the future called X-70. Now it's a practical reality. Designed into it are the features industry requested that would guarantee unprecedented efficiency at lowest possible operating costs. A president of one of Atlanta's growing companies summed it up this way, "... All our employees are excited about this lift truck because it will enable them to do a better job with

less effort. That's just plain dollars and cents value to me."

How can a fork truck cause such excitement? Test the *Clarklift* yourself and see. Just call your local Clark dealer (listed in the Yellow Pages), or write us direct, and ask for a demonstration.

Industrial Truck Division

**CLARK
EQUIPMENT
COMPANY**
Battle Creek 11,
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CLARKLIFT is a trademark of Clark Equipment Company



GRINNELL CO., INC.—"What we like most about this truck is the automatic transmission. It's a lot faster than having to shift gears . . . and it makes driving easier too," said the Warehouse Superintendent. "In tight places, for example, it's important not to have to concentrate on shifting gears. This truck's a beaut."



CAMPBELL COAL CO.—"As one of the South's largest suppliers of building materials we handle millions of board feet of lumber," says C. T. Drake, Mill Superintendent. "Our Clarks keep pace with our heavy handling needs . . . move large and awkward loads under the most adverse conditions."



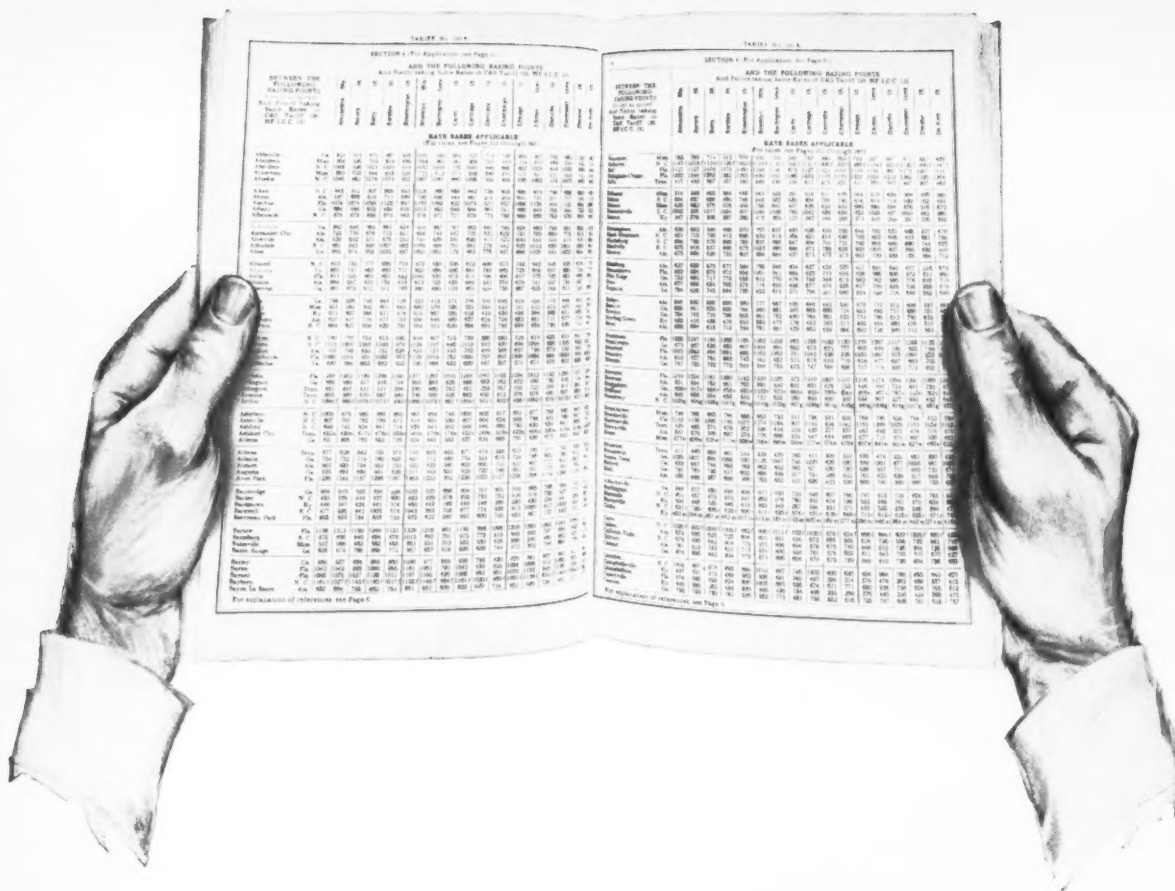
GIBSON MANUFACTURING—"Because of the nature of our product (commercial and industrial lighting fixtures) gentle handling is particularly important to us. We bought our *Clarklift* to cut expensive product damage caused by handling," reports R. R. Gibson, President. "... the *Clarklift* stood out above all others in stability and smooth performance. We're sold."



LARKIN COILS, INC.—"This truck works well in tight spots such as loading a trailer," said Bill King, Shipping Clerk. "And another thing, this truck can't run away from you. When you get off the seat, the emergency brake goes on. It's a safety feature we especially like."



SOUTHERN BONDED WAREHOUSE CO.—"We're fully palletized," said Bill Rooker, President. "Everything comes in on pallets, is handled on pallets, goes out on pallets. And we handle everything . . . auto engines, foods, chemicals, paper—you name it. Because we're so conscious of mechanized handling, we were determined to get the best. We found no rival to your *Clarklifts*."



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Coming Events

- May 12-18—National Transportation Week, sponsored by the Associated Traffic Clubs of America.
- May 13-16—Canadian Warehousemen's Assn., 40th Annual Convention, Harrison Hot Springs Hotel, Harrison Hot Springs, British Columbia.
- May 13-15—Cooperage Industries of America, Inc., Annual Meeting, Jefferson Hotel, St. Louis.
- May 14—National Transportation Institute, Transportation Assn. of America, Commodore Hotel, New York, N. Y.
- May 14-16—ATA's Council of Safety Supervisors, Baker Hotel, Dallas, Tex.
- May 15-16—Association of Interstate Commerce Practitioners, National Meeting, Conrad Hilton Hotel, Chicago.
- May 16—National Rivers and Harbors Congress, Washington, D. C.
- May 16—Hoist Mfrs.' Assn., Inc., Mayflower Hotel, Washington, D. C.
- May 17—National Defense Transportation Day, sponsored by the National Defense Transportation Assn.
- May 20-24—Regular Common Carrier Conference, ATA, San Francisco, Cal.
- May 21-23—Freight Loss and Damage Prevention Section, Assn. of American Railroads, Annual Meeting, Chateau Frontenac Hotel, Quebec, Can.
- May 22-25—Chain Store Traffic League, Annual Meeting, New Orleans, La.
- May 26-30—National Tank Truck Carriers, ATA, Annual Meeting, Sheraton-Cadillac Hotel, Detroit.
- May 28-June 1—Local Cartage National Conference, Annual Convention, Edgewater Beach Hotel, Chicago.
- June 13-14—Material Handling Conference, 2nd Annual, Society for Advancement of Management, Hotel Statler, N. Y.
- June 16-29—Material Handling Training Conference, 4th Annual Meeting, Industrial Management Center, Lake Placid, N. Y.
- June 17-19—Freight Claim Council of the ATA, Annual Meeting, Hotel Wisconsin, Milwaukee, Wis.
- June 19-20—ATA Executive Committee, ATA Headquarters, Washington, D. C.
- June 24-26—National Wooden Box Assn., Annual Summer Meeting, Lake Placid Club, Lake Placid, N. Y.
- June 24-26—Packaging Research Clinic, American Management Assn., Sheraton-Astor, New York.
- July 15-17—Truck-Trailer Mfrs. Assn., Summer Meeting, Homestead Hotel, Hot Springs, Va.

Shippers Advisory Boards

- May 20-21—Central Western, Salt Lake City, Utah.
- May 21-23—Southwest, Amarillo, Tex.
- June 6-7—Trans-Mo-Kansas, Kansas City, Mo.
- June 12-13—Southeast, Memphis, Tenn.
- June 13-14—Pacific Coast, Los Angeles, Cal.
- June 19—Ohio Valley, Indianapolis, Ind.
- June 20-21—Pacific Northwest, Seattle, Wash.

Shipper-Motor Carrier Conference

- May 14-15—Midwest Shipper-Motor Carrier Conference, 18th Regular Meeting, Congress Hotel, Chicago.

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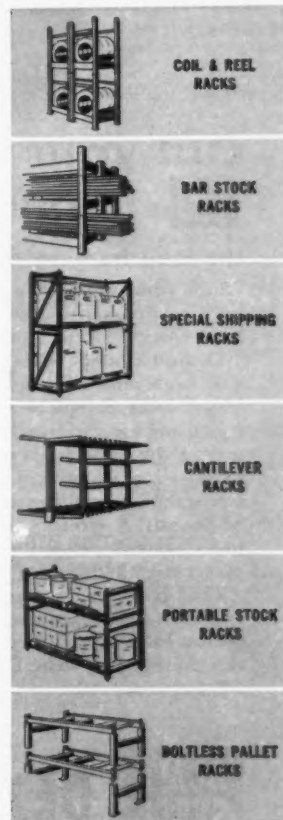
Well Stacked Racks for Industry

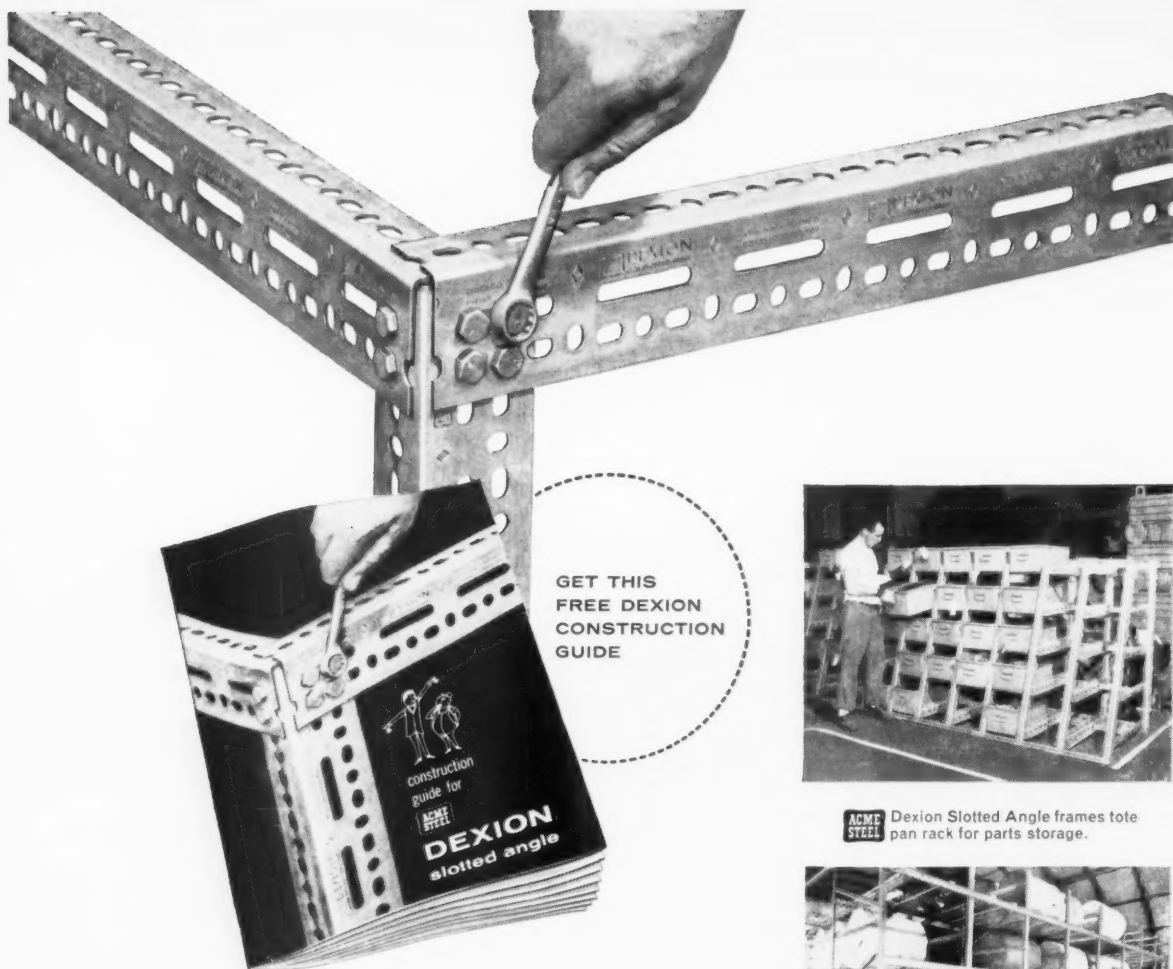
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Cut your costs with **ACME STEEL** Dexion Slotted Angle

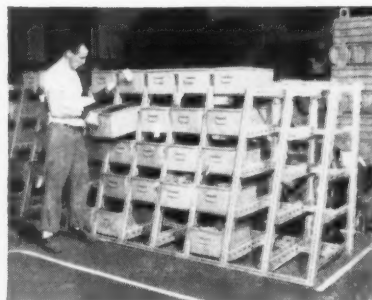
the Idea framing material for all installations

Frame whatever plant equipment you may need, including electrical installations, more easily, quickly and at reduced costs with Acme Steel Dexion Slotted Angle.

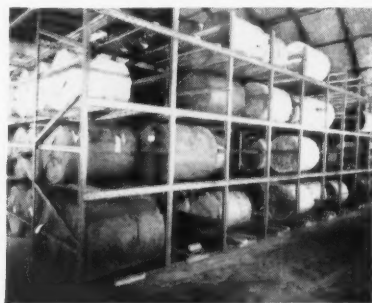
Dexion Slotted Angle measures, cuts and bolts together at the job site. Light and compact, Dexion Slotted Angle is packaged in 10-length units, complete with nuts and bolts.

For any kind of framing, Dexion Slotted Angle slot and hole patterns always match up, a special feature whether for custom building storage racks or hanging and installing electrical equipment. And strength factors allow stable, safe frameworks for holding hundreds of pounds. A wrench and the portable Dexion Cutter are the only tools needed. No drilling or welding... merely cut and bolt the pieces together to your exact specifications.

Cold-rolled, galvanized steel Dexion Slotted Angle is available in two sizes—standard and heavy duty. And it is completely reusable. A free Idea copy of the "Dexion Construction Guide" is yours for the asking. Just write Dept. DBD-57, Acme Steel Company, Chicago 27, Illinois.



ACME STEEL Dexion Slotted Angle frames tote pan rack for parts storage.



ACME STEEL Heavy-duty Dexion Slotted Angle racks weighty paper rolls.



ACME STEEL Dexion Slotted Angle used for stationary and mobile pallet racks.



DEXION SLOTTED ANGLE

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NOW! USS Gerrard **AUTOMATIC STRAPPING MACHINE**

Fully Tested, Tried and Proven



See this USS Gerrard Automatic Strapping Machine in our booth at the National Materials Handling Show, Convention Hall, Philadelphia, April 29 to May 3.

GERRARD STEEL STRAPPING DIVISION, UNITED STATES STEEL CORPORATION
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UNITED STATES STEEL

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LOW COST—This is one of the lowest-cost automatic strapping machines on the market today.

COMPACT—Requires only 11½ sq. ft. of floor space. Table height, 31". Weight (approx.), 1500 lbs.

SPEED—Up to 24 ties per minute.

TWO SIZES—Either machine will handle packages as small as 8" wide by 2" high. The larger will handle packages as large as 26" wide by 20" high.

EASY TO OPERATE—Utilizes USS Gerrard Round Steel Strapping from 14 through 18 gauge. Easy to thread and operate.

LOW MAINTENANCE—One-shot, built-in lubrication system and compact, rugged, simple construction mean minimum service and maintenance.

TENSION—Can be predetermined, and when set is uniformly maintained on all packages regardless of size or weight.

SINGLE POWER SOURCE—All functions are mechanically controlled and interlocked. Simply plug in and run.

PACKAGES which vary in size and shape can be strapped without adjustment to machine.

COMPRESSION DEVICE for pre-compressing packages or bundles can be supplied as optional equipment.

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For more complete details, contact your USS Gerrard sales representative. Also mail coupon for folder and the Gerrard Blue Book of Packaging. This contains full information on all USS Gerrard Machines and round and flat strapping.

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NEW YORK to DETROIT	5.90
DENVER to OMAHA	6.42
SEATTLE to LOS ANGELES	9.80
PHILADELPHIA to PORTLAND	24.15
SAN FRANCISCO to BOSTON	27.00

*These are the rates for many commodities. They are often lower for larger shipments. Rates shown are for information only, are subject to change, and do not include the 3% federal tax on domestic shipments.

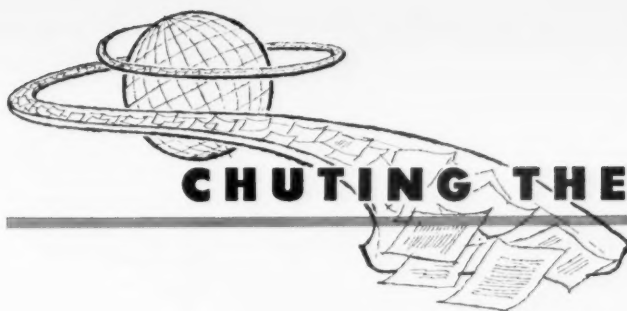


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NEWS

TTMA President Appoints Chairman for Committees

President A. A. Kearney, of the Truck-Trailer Manufacturers Association, announced appointments to the group's standing committees last month.

Chairmen are as follows: William R. Hummel, Trailmobile, Inc., engineering; Ralph Veenema, Veenema and Wieggers, Inc., finance; E. J. Lucas, Kingham Trailer Co., legislative; D. E. Sheridan, Talbert Construction Equipment Co., low bed manufacturers; Gerard H. Stein, Ohio Body Manufacturing Co., membership; L. C. Allman, The Allman Co., public relations; and A. L. Rich, Fruehauf Trailer Co., traffic.

Traffic Clubs, Associations Planning Observance of National Transportation Week

Action on both national and local levels is planned by the Associated Traffic Clubs of America in observance of National Transportation Week, May 12-18.

Frank L. O'Neill, St. Paul, president of the national organization, has stated that 128 clubs throughout the country will participate.

The Atlantic - Gulf Intra-coastal Waterway Association has been organized in Jacksonville, Fla., to promote a barge canal in North Florida which would join the Atlantic Intra-coastal Waterway with the Gulf of Mexico. It is also proposed to extend the Gulf Intra-coastal Waterway eastward to connect with the trans-Florida channel.

The Transportation Association of America, in marking the occasion, will hold its National Transportation Institute on Tuesday, May 14. Sessions will be held in the Commodore Hotel, New York, from 10 am to 4 pm.

Speakers, all basing their topics on the general theme of "Transportation in the Changing World," will be Carter L. Burgess, president, Trans World Airlines, airlines; J. L. S. Snead, Jr., president, Consolidated Freightways, Inc., highways; A. E. Perlman, president, New York Central System, railroads; and Millard G. Gamble, president, Esso Shipping Co., water carriers.

Edwin G. Nourse, former chairman, Council of Economic Advisers, will speak on "The Changing Economy We Must Serve."

NFWA Holds Election of Officers



Officers elected during the recent annual meeting of the National Furniture Warehousemen's Assn. are (left to right) Daniel P. Bryant, vice president and regional director of the western division; J. W. C. Wright, Jr., vice president and regional director of the southern division; J. C. Aspinwall, Jr., director-at-large; William J. Croul, president; Joseph A. Hollander, Sr., vice president and regional director of the central division; and Luther P. Waring, vice-president and regional director of the eastern division

LCNC Annual Convention Planned For May 28-June 1 in Chicago

The Local Cartage National Conference will hold its Annual Convention May 28 to June 1 in Edgewater Beach Hotel, Chicago.

Officials report that business meetings of Local Cartage and Short Haul Carriers will be alternated with the Heavy and Specialized Carriers, thus permitting a member to attend both if he desires.

(Please Turn Page)

Chuting the News . . .

(Continued from Preceding Page)

NITL Officials File Statements Opposing Rail Freight Increases in East, West, South

Three leaders of The National Industrial Traffic League have filed verified statements with the ICC in opposition to proposed freight rate increases.

Territories covered in the statements are East, West, and South.

Eugene Landis, chairman of a special committee to study this increase, urged the ICC to disapprove any increase in the general level of rates. He asked that authorization be withheld until it is clearly established that the railroad earnings are inadequate in terms of the statute; that the deficiency cannot be made up by savings, and that such relief will be afforded by higher rates.

J. M. Cody, a member of the NITL Executive Committee, opposed increases in rates for lcl and merchandise traffic.

William B. Saunders, League consultant and economic advisor, discusses what the railroads need and how those needs should be met.

Trucking Industry Sets Record For Intercity Freight During 1956

Tonnage of intercity freight transported by the trucking industry during 1956 was up 3.2 per cent over 1955 to set a new record.

It was the eleventh year of the past 12 in which an increase occurred. The 1956 figure resulted from relatively large increases in the first two quarters—7.9 and 5.6 per cent. These were followed by a nominal rise of 0.4 in the third quarter and a decline of 0.2 in the fourth.

The tonnage trends were disclosed in special studies by the Research Department of the American Trucking Associations, Inc. The reports covered operations of 1929 Class I intercity motor carriers of property of all types for the fourth quarters and full years 1956 and 1955.

These operators transported 291,178,873 tons of intercity freight during 1956 as compared with 282,259,180 tons in 1955.

News Briefs

The second annual Material Handling Conference, sponsored by the Society for Advancement of Management, will be held at the Hotel Statler, New York City, June 13 and 14.

Seventy-eight persons have been admitted to practice before the ICC. The list was released by the Commission on April 16.

The House Appropriations Committee in its recent report, rejected the \$322,090 funds included in the 1958 budget estimate for the Census of Transportation.

Slick Airways inaugurated a combination motor freight-air-cargo service across the nation on Wednesday, April 24.

Plans for piggy-back service to be inaugurated in the Southeast recently were announced jointly by the Florida East Coast Railway, and Great Southern Trucking Co.

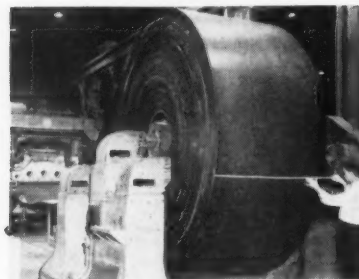
The Baltimore and Ohio Railroad's Trailer-on-Flat Car service is being extended to include Dayton, O. The extension will provide piggy-back between Dayton, New York, Baltimore, Philadelphia, Washington, Chicago, Pittsburgh, and St. Louis.

The 36th Annual Conference of the National Retail Dry Goods Assn. Traffic Group will be held May 6-8 at the Edgewater Beach Hotel, Chicago.

James F. Pinkney, General Counsel, American Trucking Assn., will appear before the Midwest Shipper-Motor Carrier Conference at its 18th regular meeting in Chicago, May 14-15. He will give the latest available information regarding the status of important legislative matters effecting transportation now pending in Congress.

A bill introduced by Rep. Frank W. Boykin, of Alabama, provides for the licensing of independent foreign freight forwarders. The secretary of commerce would be empowered to follow a system of licensing similar to that covering customhouse brokers.

Record Belting



An engineer measures the width of the largest conveyor belt ever made in one piece by B. F. Goodrich Industrial Products Co., Akron, Ohio. Over 1000-ft long and 60-in. wide, it will carry iron ore for the Orinoco Mining Co. in Puerto Ordaz, Venezuela

Western Motor Freight Claims Group Holds Meeting in Denver

The nation's motor carriers have achieved a high standard in freight claim settlements with more than 70 per cent of all claims settled within less than 30 days.

This was reported by John M. Miller, head of the American Trucking Associations Freight Claim Section, during a two-day meeting of the Western Motor Freight Claims Conference held in Denver.

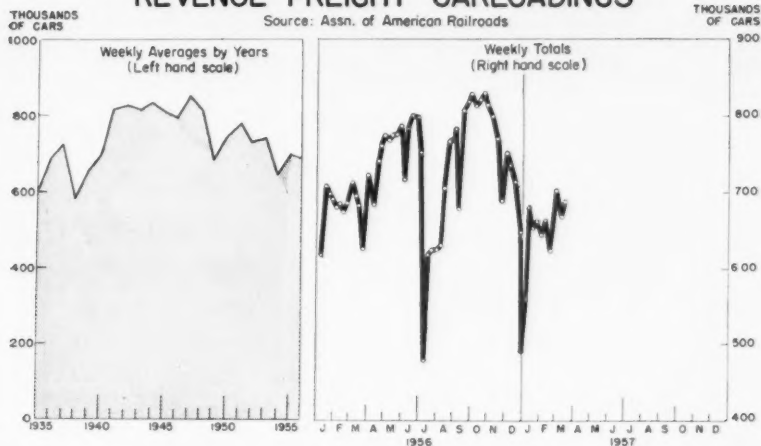
Miller criticized legislation now before Congress to impose ICC control over settlement of freight damage or loss by interstate motor carriers.

Special Truck Taxes Reported At Near \$2-Billion for 1955

Owners of private and for-hire trucks in 1955 paid \$1.9-billion in special federal, state, and local motor vehicle taxes, according to "American Trucking Trends."

This figure, it is reported, does not include property and income taxes. American Trucking Associations, Inc., publisher of the newly issued annual volume, estimates that the 1956 totals for these taxes will top the \$2-billion mark. Truck taxes in 1957 and years to follow will increase even more sharply as a result of the new highway program, according to ATA officials.

REVENUE FREIGHT CARLOADINGS



Greenway Renamed President By Folding Carton Corp.

Norman F. Greenway, vice president and general manager of the Boxboard and Folding Carton Division, Robert Gair Group, Continental Can Co., Inc., was re-elected president of the Folding Paper Box Association at the group's recent Annual Meeting.

In addition to the president, members of the Executive Committee will be: William J. Alford, 3rd, Alford Cartons; William B. Leavens, Jr., Wilkata Folding Box Co.; Arthur N. Morris, Newth-Morris Box Corp.; Bayle M. Richardson, Richardson Taylor-Globe Corp.; Leo H. Schoenhofen, Container Corp. of America; and William H. Walters, U. S. Printing and Lithograph Co.

—PA—

Fibre Box Association Picks Theme for National Competition

"New Directions in Packaging" will be the theme of the 1957 Fibre Box Competition to be held in Washington, D. C., October 17 and 18.

The latest developments in corrugated and solid fibre shipping containers will be featured, according to Albert W. Luhrs, executive manager of the Fibre Box Association, sponsoring organization. Manufacturers are expected to enter more than 500 different examples of their products.

Motor Common Carriers Merge As Spector-Mid States

Two large motor common carriers—Spector Freight System, Inc., and Mid-States Freight Lines, Inc.—have merged in a company which will serve points in all 48 states.

The new company will be known as Spector-Mid States while the corporate name will be Spector Freight System, Inc. The new firm's fleet includes 3500 pieces of equipment to be operated over more than 15,300 miles of operating rights.

President following the merger is W. Stanhaus, who had been president of Spector. Mid-States' president, Cecil Vernon, is executive vice president of the new firm. The new organization will have its headquarters in Chicago.

—DA—

The American Society of Traffic and Transportation has added the following new members: William A. Carson, Texas Gulf Sulphur Co., Inc.; Arthur R. Hartzell, Jr., Charles Pfizer and Co., Inc.; George H. Hubbard, Kennecott Copper Corp.; Maj. Donald Y. Huff, U. S. Air Force; Cecil H. Hull, Jr., Diamond Alkali Co.; William Langer, Art Steel Co.; Robert H. Russell, Interstate Motor Freight System; and L. M. Shinn, Interstate Commerce Commission.

Legislative Line-up

APPROPRIATIONS—Senate Appropriations subcommittees are holding hearings on the Commerce Dept. (HR 6700) and Labor Dept. (HR 6287) requests for new funds. Treasury, Post Office and Interior Dept. bills are not yet considered.

FOREIGN FREIGHT FORWARDERS—House Merchant Marine Committee has started no action on HR 479, to require licensing of foreign freight forwarders by the Federal Maritime Board.

GOVERNMENT COMPETITION—HR 1975 still awaits action by a House Government Operations subcommittee. The bill would state federal policy on the extent of federal competition with private business.

HIGHWAY BUILDING—A Senate Public Works subcommittee has completed hearings on the national highway program. No action is started in the House on bills to extend the interstate road system.

MINIMUM WAGES—Hearings on bills to extend the minimum wage law are going on before a House Labor subcommittee. These hearings probably will last through May. Senate Labor subcommittee has concluded hearings on bills to change the law.

TRUCK ROUTES—House Commerce subcommittee is holding hearings on HR 1066, to require "reasonable" through rates for common carrier truck lines.

TERMINAL TRAFFIC—Hearings were expected to start before a House Merchant Marine group in late April on HR 2229. This bill would require more government-generated traffic to pass through commercial marine terminals.

TRANSPORT POLICY—Bills to revise the national transportation policy in detail (S 1457 and HR 5521) are dormant at this time.

—DA—

National Truck Leasing System Holds Spring Meeting in Florida

Top management men representing leasing companies from throughout the nation were present for the 1957 Spring Executive Conference of National Truck Leasing System, held recently in Florida.

Round tables were held on management problems, depreciation, regulations, rental procedures, and shop economies.

Officers of the group are John Black, Jr., president; Frank Max, Jr., treasurer; and Martha Dunlap, executive director.

(Please Turn to Page 16)



New A-Line models range from ½-ton Pickups through 33,000 lbs. GVW Six-Wheelers.

NEW ACTION-STYLING! MORE USABLE POWER!

Here is the crowning achievement of fifty years of quality truck production—the great new Golden Anniversary INTERNATIONAL Trucks!

They're *Action-Styled* with fresh, clean functional lines that set a new style pace.

They're powered by new engines that put out more *usable* horsepower—including the most powerful "six"

available in its field! They have exclusive new cab mountings for quieter, more level ride. New brakes, new steering, new frames—and many other new features.

The result is a new line of trucks that—more than ever before—are built to cost *least* to own!

See and drive these newest INTERNATIONALS *today!* International Harvester Company, Chicago.

Trucks Unlimited...Powered for Modern Traffic...Plus Modern Comfort

Handsome "Golden Jubilee" Pickup with the *longest* all-steel body in its class. Only Panel with third door. New 8-passenger Travelall® models. New cab-forward models with ideal 89-inch BC dimension. Tractors to 48,000 lbs. GCW. Wide range of all-wheel-drive trucks.

Redesigned engines produce *increased* power without strain from new combustion chamber and valve position . . . more usable power that's "bred for the job". . . at low rpm to keep operating and maintenance costs low. New quick-starting 12-volt ignition.

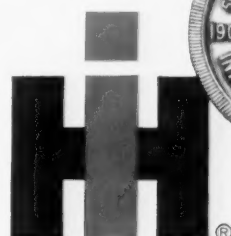
Biggest windshield—1,181 sq. in.—and *widest* cab in their class! New "Silent-Vent" door wings. New, wider front and rear springs. Exclusive level-riding 5-point cab mounting. Bigger brakes with more lining area, larger cylinders and boosters for quicker, easier stops.



Other INTERNATIONALS, to 96,000 lbs. GVW, round out the world's most complete line.

NEW Golden Anniversary **INTERNATIONALS**

Cost least to own!



Motor Trucks • Crawler Tractors • Construction Equipment
McCormick® Farm Equipment and Farmall® Tractors
Circle No. 6 on Card, Facing page 51, for more information

MAY, 1957

15

Chuting the News . . .

(Continued from Page 13)

MHI Chairmen, Officers Meet



Committee chairmen and officers of the Material Handling Institute, Inc., conferred at the spring meeting recently in Chicago. Shown seated (left to right) are John G. Bucuss, chairman, Management Group Committee; George G. Raymond, Jr., president; Eugene Caldwell, second vice president; and R. K. Lohman, chairman of the Professional Society Committee. Standing are George Greenberger, chairman, Membership Committee; E. W. McCaul, chairman, Technical Sessions Committee; D. H. Bitney, vice chairman of the College-Industry Committee; Ward E. Duchene, public relations chairman; D. H. Cissna, Social Committee chairman; and C. O. Hedner, vice chairman, Education

Workshop Seminar Sessions Held by Eight Committees During Chicago Meeting of MHI

Workshop seminar sessions were held by eight committees during the recent Chicago meeting of The Material Handling Institute, Inc.

Committees which met and their chairmen are: Membership, George Greenberger; Exposition, Roland Whitehurst; Technical Sessions, E. W. McCaul; Social, David H. Cissna; MHI Educational, C. O. Hedner (vice chairman); Professional Society, R. K. Lohman; Management Group, John G. Bucuss, and Public Relations, Ward E. Duchene.

President George G. Raymond, Jr., explained during the meeting that the groups' two major fields of activity—membership and markets—will be under direction of First Vice President R. L. Fairbank and Second Vice President Eugene Caldwell.

The fourth annual Material Handling Training Conference is scheduled this year from June 16-29 at the Industrial Management Center, Lake Placid, N. Y.

Wirebound Container Makers Set All-Time Record During 1956

The wirebound shipping container industry, in 1956, set an all-time record for the third successive year with a total volume of \$120,660,973.

The new record was reported at the recent annual meeting of the Wirebound Box Manufacturers Association in Chandler, Ariz.

During that meeting, John R. Miller, Jr., of T. R. Miller Mill Co., was re-elected president. New directors are Stanley C. Craven, Crosby Forest Products Co., and Al J. Sroub, American Box Co.

Club Briefs

Los Angeles Transportation Club was host aboard the SS Shearwater for a tour of the Port of Long Beach and Port of Los Angeles on April 30.

Traffic Club of Kansas City installed new officers at its 37th annual dinner last month.

North Jersey Traffic Club sponsored Perfect Shipping Nite, April 8. Fifty-five diplomas were presented to men who successfully completed a traffic management course sponsored by the Educational Committee.

Edward J. McCabe, Waterman Steamship Corp., was guest speaker at the April 1 meeting of Delta Nu Alpha Transportation Fraternity, Philadelphia Chapter.

Metropolitan Traffic Assn. of New York, Inc., held its 31st Annual Banquet at Hotel Commodore, May 1.

Edward F. Bowes, senior partner, Bowes & Millner, formerly Bureau of Motor Carriers, ICC, was guest speaker at a recent meeting of the Metropolitan New York Chapter, Association of ICC Practitioners.

New officers of the Ohio State Industrial Traffic League are: F. J. Gale, Globe Steel Abrasive Co., president; C. C. DeVelder, Ford Motor Co., first vice president; G. R. Hallett, Wakefield Co., second vice president; William J. Hirsch, Lincoln Electric Co., secretary, and R. J. Joyce, Central Ohio Paper Co., treasurer.

The Traffic Club of Houston will hold its 42nd annual banquet and inaugural ball in the Emerald Room, Shamrock-Hilton Hotel, May 24.

The Traffic Club of Syracuse hosted the Atlantic States Shippers Advisory Board, their members and friends at meetings held at the Hotel Syracuse, April 10-11.

Bay Area Chapter, American Materials Handling Society, held its regular monthly meeting at Hotel Whitcomb on April 4. Arthur Schulz, consultant in warehouse operations, Safeway Stores, Inc., was guest speaker.

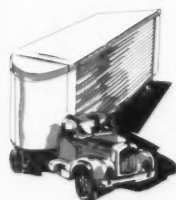
Clarence G. Morse, Federal Maritime Administrator, Washington, D. C., addressed the annual luncheon meeting of the Marine Exchange, Inc., World Trade Center, San Francisco, on April 4.

Joseph F. Finnegan, director, Federal Mediation and Conciliation Service, Washington, D. C., was guest speaker at the 13th annual Stevedore and Terminal Operators night, Foreign Commerce Club of New York, Inc., on April 16.

Proposed plans for the acquisition and financing of new railroad rolling stock were discussed at a special meeting of the Board of Directors of the Association of American Railroads recently.

(Please Turn to Page 112)

T*he dominant
source of service
and information
on bona fide*



THIS CARD TEARS OUT

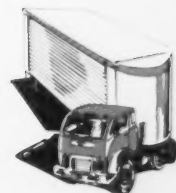
It requires no postage. It will bring you facts about truckleasing. First, read the other side of this page for general information about true, all-service NTLs truckleasing, then send the card for specific facts relating to your problem.

T *RUCKLEASING:*

THE NATIONAL TRUCK LEASING SYSTEM, a nationwide network of the oldest and most experienced companies in the truckleasing business. NTLs headquarters annually receives thousands of requests to explain the advantages of true all-service truckleasing — everything supplied but the driver—as contrasted with the many other variations of “leasing” contracts and services such as “finance,” “driver-owner” deals, and “trip-leasing.”

These inquiries come from traffic executives of major corporations, law-makers and regulatory bodies, certified public accountants, attorneys, and others who want to clarify current terminology and contractual arrangements covered by all-service truckleasing.

This authoritative position maintained by National Truck Leasing System in the industry is not only because the System constitutes a group of highly qualified service concerns located coast-to-coast, border-to-border, but also because it has no other single purpose than the impartial promotion and supplying of true, all-service truckleasing.



BUSINESS REPLY CARD

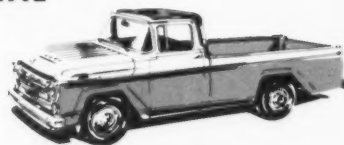
FIRST CLASS PERMIT NO. 32084 SEC. 34 D P. L. & B. CHICAGO, ILL.

**NATIONAL TRUCK LEASING SYSTEM
23 EAST JACKSON BLVD.
CHICAGO 4, ILLINOIS**



W

hat are the advantages to **YOU** IN LEASING YOUR TRUCKS FROM THE NATIONAL TRUCK LEASING SYSTEM?

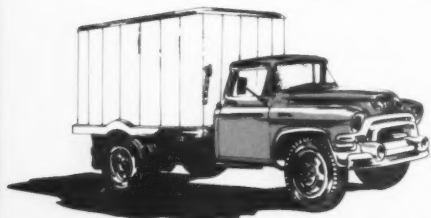


NTLS's true all-service truckleasing furnishes everything...everything but the driver. Your own trucking problem is different from that of any other firm. Before you lease your trucks, you may want to know answers to such questions as, "What is done with the trucks you now own?" "Is leasing more advantageous for a small fleet (even one truck) or a large fleet?" "Who washes the trucks?" "What kind of contract does an NTLS company give you which supplies insurance, licensing, constant maintenance, extra trucks for peak seasons or other reasons . . . in fact everything but the driver?" All these services and more, are provided in a single NTLS trucklease "package."

Other important advantages include the saving of your executive time, worry, and head-aches in procuring and maintaining trucks in addition to operating your business . . . when you lease you are assured uninterrupted service . . . competent engineering of your trucks.

Very important, too, is the release of frozen capital—especially in a tight money economy but, even when money is easy to obtain, why borrow or freeze your capital to add to your problems? NTLS true, all-service truckleasing furnishes everything . . . everything but the driver.

...ALL IN ONE INVOICE...COSTS KNOWN IN ADVANCE...WHOLLY TAX-DEDUCTIBLE



The postcard tears out. It requires no postage. It implies no obligation. Send it today.



NATIONAL TRUCK LEASING SYSTEM
23 East Jackson Blvd., Chicago 4, Illinois

Gentlemen:

☐ I am interested in more detailed information and the name of the nearest NTLS company.

We now use and operate _____ trucks, based at _____

☐ I am also interested in daily truck rental services.

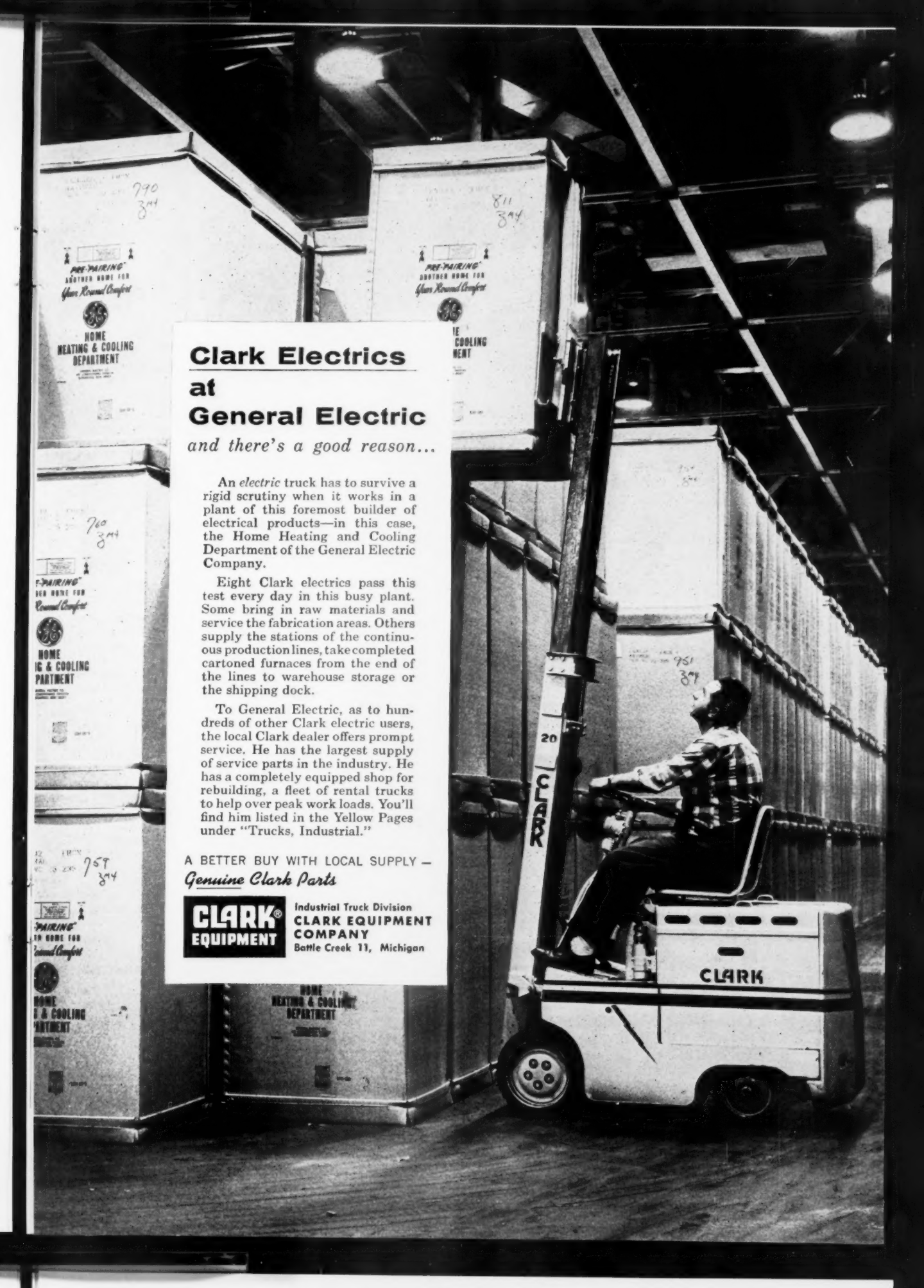
NAME _____ POSITION _____

FIRM _____ ADDRESS _____

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**TRUCK LEASING
SYSTEM**

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Members in principal cities



Clark Electrics at General Electric

and there's a good reason...

An electric truck has to survive a rigid scrutiny when it works in a plant of this foremost builder of electrical products—in this case, the Home Heating and Cooling Department of the General Electric Company.

Eight Clark electrics pass this test every day in this busy plant. Some bring in raw materials and service the fabrication areas. Others supply the stations of the continuous production lines, take completed cartoned furnaces from the end of the lines to warehouse storage or the shipping dock.

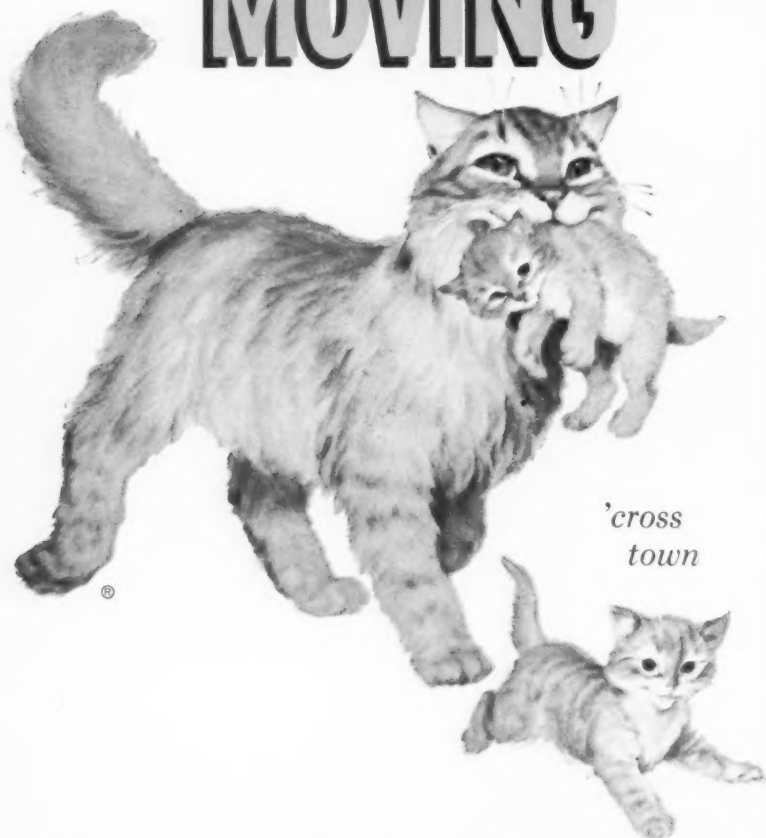
To General Electric, as to hundreds of other Clark electric users, the local Clark dealer offers prompt service. He has the largest supply of service parts in the industry. He has a completely equipped shop for rebuilding, a fleet of rental trucks to help over peak work loads. You'll find him listed in the Yellow Pages under "Trucks, Industrial."

A BETTER BUY WITH LOCAL SUPPLY —
Genuine Clark Parts

**CLARK®
EQUIPMENT**

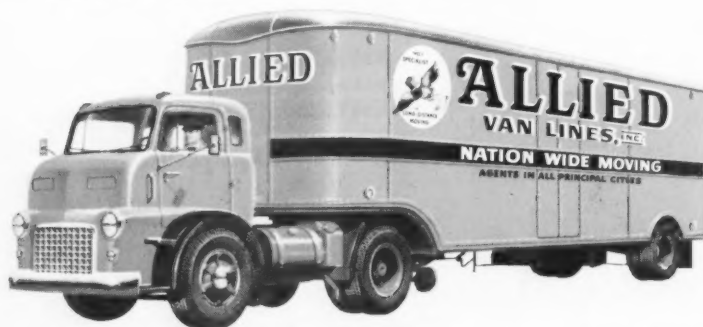
Industrial Truck Division
**CLARK EQUIPMENT
COMPANY**
Battle Creek 11, Michigan

for **DEPENDABLE** **MOVING**



CALL YOUR ALLIED MOVER ...he's the No.1 Specialist

Ask your Allied Mover for a copy of "Before You Move." See yellow pages of your phone book, or write Allied Van Lines, Inc., Broadview, Illinois.



ALLIED VAN LINES • WORLD'S LARGEST LONG-DISTANCE MOVERS

MEN **IN THE NEWS**

Traffic

W. R. Bonner—appointed traffic manager, Victor Products Corp., Hagerstown, Md.



Stephen L. Meyer—new general traffic manager, Joseph E. Seagram & Sons, Inc., New York, N. Y.

Harry E. Nelson, Jr.—named traffic manager, Doughboy Industries, Inc., Richmond, Wis.

J. D. Patterson—appointed vice president and general traffic manager, Union Bag—Camp Paper Corp., New York, N. Y.



Edwin A. Olson—general traffic manager of Libby, McNeill & Libby, Chicago, named general traffic and warehousing manager.



William H. Montgomery—new director of traffic, Riegel Paper Corp., Milford, N. J. **Robert J. Helms**, promoted to traffic manager, New Jersey plant.

Edwin Avery—traffic manager, Detroit Port Commission.

Colonel Morton E. Townes—promoted to Brigadier General in the Army. He is currently serving as U. S. Army Assistant Chief of Transportation (Traffic) in Washington, D. C.

Transportation—Air

Shepard Spink—elected vice president, Merchandising, American Airlines, New York, N. Y.

Frank J. Beach—appointed director, Airfreight traffic, American Airlines, New York, N. Y.



James M. Mask—appointed director of Purchasing, Slick Airways, Dallas, Tex.

—Highway

Michael J. Moore—promoted to assistant district sales manager, New York City area, Denver-Chicago Trucking Co.

J. L. S. Snead, Jr.—president, Consolidated Freightways, Inc., elected chairman of the Board of Governors of the Regular Common Carrier Conference.



Ray J. Fellows (left)—appointed sales manager, Wisconsin Motor Corp., Milwaukee. **F. Burrows Esty** (right)—chief engineer.

George D. Sax, Cecil Vernon and **Julius Epstein** elected to the Board of Directors, Spector Freight System, Inc., Chicago.

John R. MacDonald—appointed general traffic manager, Chippewa Motor Freight, Inc., Eau Claire, Wis.

Ralph M. Buzard—appointed general manager; **Louis W. Pierson**—promoted to manager of sales and **William E. Callahan**—appointed assistant sales manager, International Harvester Co., Motor Truck Div., Chicago.

—Rail

E. M. Talbott—appointed assistant comptroller, Baltimore & Ohio Railroad Co., Baltimore, Md.

Milton G. McInnes—elected a director of the Erie Railroad, Cleveland, Ohio.

Materials Handling

John A. Metz, Jr. and **Richard E. Whitney**—elected directors of Link-Belt Co., Chicago, Ill.

Robert L. Brown—new sales manager, gasoline and LP-Gas powered industrial trucks and **Charles E. Howard**—new sales manager electric powered trucks and truck attachments, The Yale & Towne Mfg. Co., Philadelphia, Pa.

Gifford Kittredge—appointed manager of Commercial Division, Lamson Corp., Syracuse, N. Y.

Marvin Van Dusen—named to the new position of sales coordinator, Powrwork Section, Industrial Truck Div., Clark Equipment Co., Battle Creek, Mich.

The truck with "boarding house reach"

INCREASES WAREHOUSE CAPACITY

... as much as 50%



Reach-Fork

TRADE MARK

3000 lb.
CAPACITY

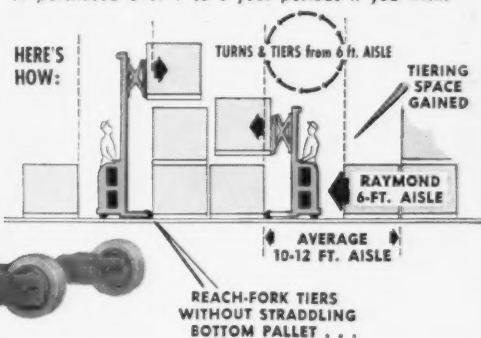


WHY BUILD OR LEASE additional storage when there's space to be had right in your present warehouse? A Raymond Reach-Fork... the truck with "boarding-house reach"... may increase your warehouse capacity up to 50%.

THE REACH-FORK eliminates wasteful 10 to 12-ft. aisles... permits you to tier in space-saving 6-ft.* aisles. No special racks or pallets required because it operates from the aisle, without straddling bottom pallet. Its forks extend to pick up or deposit your load... retract in seconds.

IDEAL FOR LOADING and unloading. Stacks pallets closely on racks, in truck trailers, boxcars, bulk storage areas. Safe on elevators and low-capacity floors. Raymond power unit is highly accessible, opens up like a book for ease in servicing. Reach-Fork available in 2,000 or 3,000 lb. capacities... may be leased or purchased over 1 to 3-year periods if you wish.

HERE'S
HOW:



MODEL E3RT Reach-Fork
Electric Tying Truck. Tele-
scopic model, 3,000 lb. cap.

*For exact aisle width, ask Raymond representative to survey your operation.

THE RAYMOND CORPORATION

13931 Madison St., Greene, N.Y.

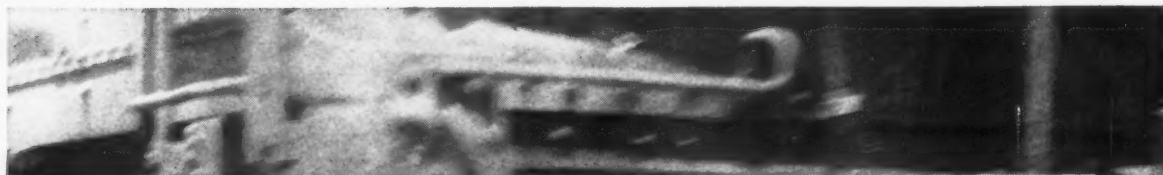
Please send Bulletin on Raymond Reach-Fork. I am interested in:

- ☐ 2,000 lb. cap. Reach-Fork.
- ☐ 3,000 lb. cap. Reach-Fork.
- ☐ Have a representative call.

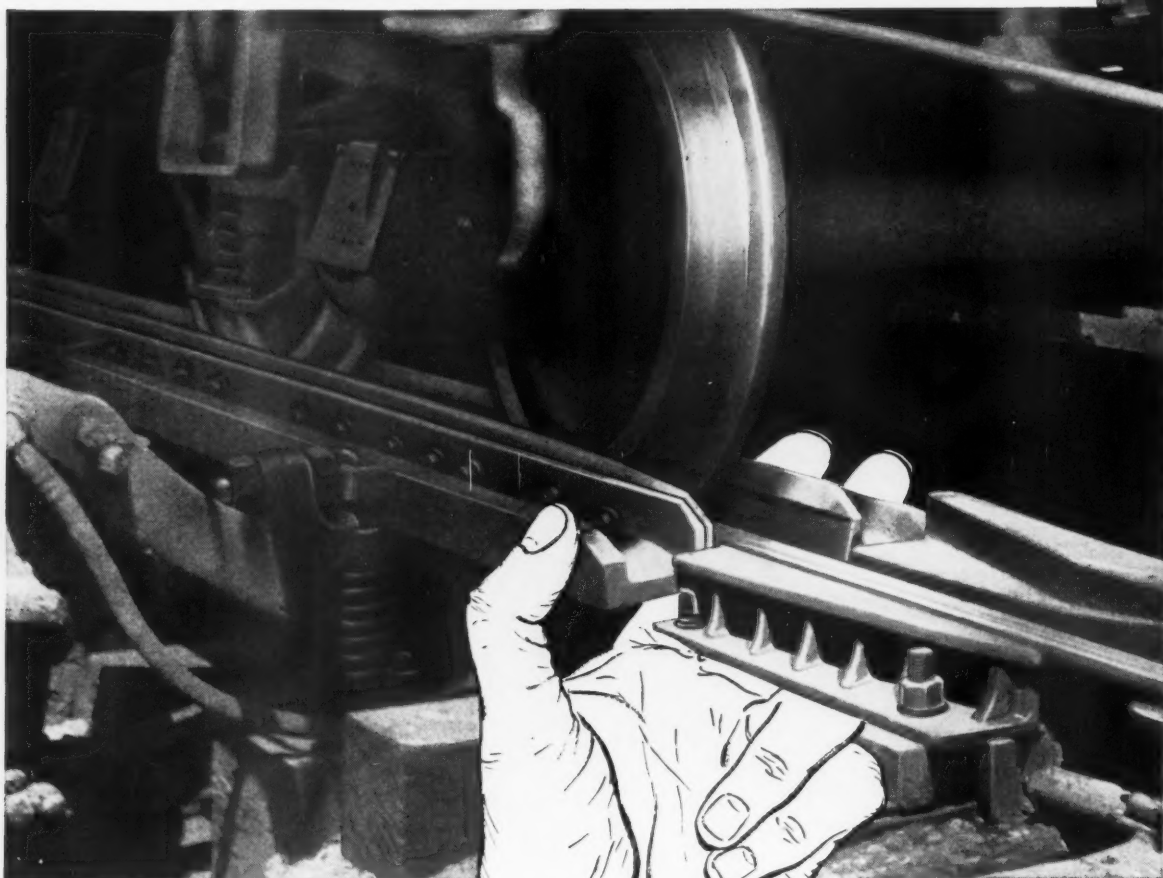
MAIL
COUPON
TODAY

NAME _____ TITLE _____
COMPANY _____
STREET _____
CITY _____ STATE _____

Circle No. 7 on Card, Facing Page 51, for more information



a SQUEEZE sets the speed



so freight cars couple into the train
with a light touch in Union Pacific
electronic classification yards

Gripping the sides of the wheels like sensitive brakes,
a carefully controlled squeeze sets the speed of rolling
cars in the electronic retarder yards of Union Pacific.

Our enviable record of damage-free handling of
freight is "proof of the pudding."

This rapid *and safe* handling of cars is another rea-
son freight rolls fast all the way when it is routed via
Union Pacific.



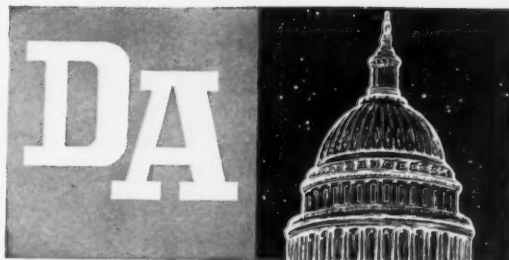
YOUR FREIGHT

MOVES

ON

**UNION
PACIFIC**

WASHINGTON



By Ray M. Stroupe, *Chilton Washington News Bureau*

HIGHWAY FUND TAPPED—Evidence of a raid on federal highway money alarms Congress and highway user groups. The U. S. Labor Dept. seeks \$365,000 from the highway trust fund. Its apparent use, says Rep. Boggs, D., La., would be for minimum wage work related to the highway program. He strongly condemns this project, emphasizing that by law only the Bureau of Public Roads is to draw administrative money from the fund.

SEEK RATE REDUCTION—General Services Administration is pondering an invitation from Senators Ives and Javits. The New York Republicans ask GSA to back a plea to the ICC by railroads serving the Port of New York for lower rates on all import-export freight. GSA controls the movement through New York of about 100,000 tons a year, the senators figure.

IMPROVED AIRCRAFT DESIGN—Planes that may carry the freight and passengers of the future give one federal agency a lively time. CAA must consider 25 new turbine-powered aircraft for certification in the next three years. Of these, 11 are foreign-built.

FIGHT SECTION 22 CURB—Pentagon officials dispute an ICC-backed bill to limit ratemaking under Section 22 of the Interstate Commerce Act. It would prohibit carriers from offering the government special rates in peacetime. Military traffic must move speedily in both peace and war, a Defense Dept. spokesman contends. Before Congress, he estimates that loss of special ratemaking could add \$128 million a year to military shipping costs.

TRANSPORTATION RESEARCH—Special transportation research tasks are outlined to the U. S. Agriculture Dept. The transportation research advisory committee to the agency considers these needs urgent: Broader fact-finding to improve shipping containers; more basic work on performance standards for trucks and truck-trailers that transport frozen foods; a start on studies of operating costs of trucks hauling farm products.

PAPERWORK DISTRIBUTION—Those 63 copies asked on any document filed in a general rail freight case are used, the ICC insists. Sen. Williams, R., Del., who inquires why so many copies are called for, gets this explanation from Chairman Clarke: Twenty-five go to the 11 commissioners and to staff officials. Another 25 are turned over to the general counsel for railroads concerned. One copy is sent to each of the 13 ICC regional offices.

STUDY FUEL CARRIERS—Petroleum industry executives will survey all domestic means for moving oil and oil products. Oil and Gas Division, U. S. Interior Dept., wants to know the findings. A committee on petroleum transportation, named by the National Petroleum Council, will check on pipelines, barges, tank cars, and tank trucks.

RAIL SPENDING TO MOUNT—Road and equipment outlays of Class I railroads this year are expected to top those in 1956 by \$175 million. Forecasts supplied to the ICC put spending of this type in 1957 at \$1.36 billion. Most marked increase is to be in the South, where a rise of almost 45 per cent is projected. Pocahontas region lines intend to boost spending by 37 per cent.

FIX MINIMUM RATES—New minimum truck rates for iron and steel go into effect in the East (and west to Iowa) by July 1. In setting the "floor" fees, the ICC predicts some increases, but no reductions, in going rates. Bottom rate for hauling a 30,000-lb load 10 miles or less is 10¢ per 100 lb. It moves up to 23¢ for trips of 100 miles. Minimum for a 500-mile trip is 70¢ per 100 lb.

HUNT OIL STORAGE SPACE—Private oil storage sites away from U. S. coastal areas are eyed by the armed forces. Storage capacity for about 31 million bbl is now under military contract, but the Military Petroleum Supply Agency wants more. It also is looking for added commercial drum-filling capacity on the Gulf and West Coasts. Interested private operators can enter their names on the MPSA source list of qualified bidders.

ICC APPEALS EXEMPTIONS—Bound for the U. S. Supreme Court is an ICC appeal for a decision on farm commodity exemptions. Involved is the Frozen Foods Express case. In it, a federal court in Texas set aside in part an ICC order covering exemptions. ICC will argue that truck movement of quick-frozen fruits and vegetables, dried egg powder and egg yolks, powdered milk, and buttermilk are not exempt, under the ICC Act.

TRUCKS ADD TONNAGE—Intercity common carrier truckers made haulage gains in January. Freight volume of carriers surveyed by American Trucking Assns. was 3.7 per cent ahead of that for January, 1956. Operators moved more than 4.6 million tons of freight this past January. All national areas except New England recorded gains. Largest percentage rises were in the Rocky Mountain, southwestern, and southern regions.



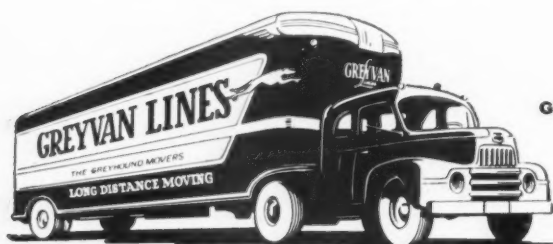
It's somebody you know...

MAYBE YOU have never done business with Greyvan long-distance movers. But you do know Greyhound. You know the policies that made Greyhound famous:

- Careful, courteous drivers
- Clean, modern equipment
- On-time service
- Scrupulous fairness in all dealings

GREYVAN operates on the same principles. We try to be less concerned with *how much* we do than with *how well* we do it. That's why, on *any* long-distance moving job, large or small, more and more Traffic Managers recommend:

- "Give it to GREYVAN, to be SURE!"



GREYVAN LINES INCORPORATED

General Office: 57 West Grand Avenue, Chicago 10



THE GREYHOUND MOVERS

LONG-DISTANCE MOVING AND STORAGE

On the line— **Fiscal Contribution**



The most outstanding fact about current corporate financial reports is that the majority say that business is as good or better than for previous comparable periods, but profits are smaller. Management attributes this to higher cost of doing business—wage increases, higher raw material costs, boosts in the costs of tools and equipment, and higher distribution costs.

Just how long this situation will continue is hard to predict. Meanwhile, management is seeking a solution on two fronts—sales and production.

In production, for example, more processing is being controlled by automation. The goal is elimination of manual operations and acceleration of production. The reports are encouraging.

But this is a slow process. Much experimentation is necessary; which slows or stops production. It requires costly modification of production machinery or complete replacement. And there are cases where automation is impossible with existing plant layout; which means costly building changes, new additions or relocation.

While this is going on, we know of cases where great and immediate operation savings can be accomplished by much less costly mechanization. Goods produced at high speed, with great efficiency and minimum labor are bottlenecked at the end of the production line. They are stacked in trucks, freight cars and storage areas at low speed, poor efficiency, and excessive manpower. This is wasting many tens of thousands of dollars annually, which could be converted readily into profits.

If you have such problems, call in an expert. Every materials handling equipment manufac-

turer has engineers on his staff who will be happy to supply you with ideas.

It doesn't matter if you don't know whose idea is best. Submit them all to the big boss. Let him decide. The important thing is to bring them all to his attention.

We know one chap who makes a practice of taking materials handling equipment salesmen through his plant. Every time one of them comes up with an idea, he submits it to the front office. Even though he can't tell the difference between a chuck and a jig, top management considers him a materials handling engineer as well as an able traffic manager.

Another profit loser is the average company warehouse. The plant's primary business is manufacturing, not warehousing. Warehouse space is unproductive. Therefore, it should be held to the minimum.

It's cheaper, always, to use public warehouses for all but in-process storage. For an immediate contribution to your company's fiscal problems, call in a public warehouseman and ask him to give you a savings estimate.

Potentially, there are more opportunities for reducing costs in distribution than in any other department. Top management isn't aware of that because, literally, traffic and warehousing are neglected and forgotten functions in the average industrial plant.

As never before, top management is desperately seeking means of cutting costs. As never before, you have a great opportunity to make a fine fiscal contribution—with profitable recognition for yourself and your department.

A. W. Greene
EDITOR

Talks to Talk

A sneaky, scheming assistant department manager, bent on gaining fast recognition, wrote a letter to the company's president.

"If you want to save \$10,000 a year, fire my boss. All he does is play golf. I do all the work."

The president called the department head on the phone and sug-

gested that the assistant be fired at once. When the manager asked why, the boss said, "He's trying to break up our foursome."

WEEPERS: Another prexy, fearing stockholders' reaction to the financial statement, asked the treasurer, "What can we do to make it look better?"

"Don't publish the figures."

"But what will I say when they

ask where the profits are?"

"How about 'finders keepers'?"

WORRY: "What are you worrying about? This is the best financial statement we've ever had," said still another president.

Another Treasurer: "Yes, but what will the stockholders say when they learn all our profits came from investments—stocks in our competitor's business?"



Drivers get set to "hit the road" with the first of the 1000 new Power Giants ordered by Ryder System, Inc.

Dodge awarded 1000-truck order



"We picked these Dodge Power Giants to give our truck-lease customers the finest hauling and delivery operation at lowest cost", James Ryder (right), president of Ryder System, Inc., tells Lee F. Desmond, vice president of Dodge.

**Ryder System, Inc., world's largest exclusive truck-leasing company,
puts low-cost operation first...picks Dodge for record order**

When your business is leasing trucks, there's only one way to make it pay off. And that's by keeping your operating costs per mile at rock-bottom levels. That's why Jim Ryder, president of Ryder System, Inc., decided on Dodge when he needed new trucks. He knew that Dodge trucks are built to take extra miles without extra costs.

For instance, new Dodge *Power Giant* V-8's are the most powerful of the low-priced three. And that extra power lets you handle the roughest hauling jobs with less engine strain. Less strain means less wear and, of course, fewer repairs. Exclusive Power-Dome design delivers premium performance on regular gas, too. "I save money both ways", says Jim

Ryder. "But more important, I know that my customers will be thoroughly satisfied with any Dodge *Power Giant* they lease."

Why not do as Jim Ryder did . . . check into the facts with your Dodge dealer. You'll find a Dodge *Power Giant* will pay off for you, whatever your business.

DODGE

PowerGiants

Most Power of the Low-Priced 3

Circle No. 8 on Card, Facing Page 51, for more information

DISTRIBUTION AGE

DEPENDABILITY



A typical 100 lb. shipment from New York to Chicago costs only \$7.50 . . . from Chicago to San Francisco only \$18.80.

with the greatest dependability in airfreight, American serves traffic managers better than any other airline!

Specify American Airlines Airfreight—the carrier that offers the best assurance of expert handling and on-time deliveries. American offers this greater dependability because American leads all other airlines in experience, capacity, coverage of key cities and frequency of schedules.

AMERICAN AIRLINES AIRFREIGHT

—flies more freight than any other airline in the world

A Mayflower Move Is a Good Move!



Whenever you have personnel to move for your company, you'll find they like Mayflower Service. Top executives, salesmen, engineers, research people . . . whatever their position . . . report complete satisfaction with Mayflower. For example, here's what a recent shipper, whom we moved from Covington, Ohio to Yuma, Arizona had to say:

"Not one thing was lost, broken or damaged. I was very satisfied in the moving of my household goods, and I shall recommend your company."

Next time you have personnel to move, call Mayflower, won't you? It's a mighty good move!

AERO MAYFLOWER TRANSIT COMPANY, INC. • INDIANAPOLIS



**AERO
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NATION-WIDE
FURNITURE MOVERS

AMERICA'S FINEST LONG-DISTANCE MOVING SERVICE

LETTERS

TO THE EDITOR

Fork Lift Makers

To The Editor:

We would appreciate receiving a list of manufacturers of fork lifts. Perlmuter Building Supply Co. Monticello, N. Y.

In its February issue, DA publishes a Where-To-Buy Directory, which includes not only names and addresses of fork lift manufacturers but also suppliers of many other types of distribution equipment. In addition, DA publishes a listing of Industrial Truck Specifications. This material, covering high and low-lift fork trucks, towing tractors, straddle carriers, and front-end loaders, will appear in the June issue. The Editor.

Efficient Palletizing

To The Editor:

Sometime ago, it is believed, your magazine printed a story regarding efficient palletizing. The story included a chart which, if one knew the dimensions of the cases, would give him the most efficient arrangement of those cases on a standard size pallet.

Would you please advise if back issues or reprints are available.

Gordon Swaney

Assistant General Sales
Manager

La Choy Food Products
Archbold, Ohio

The material you refer to was written by D. O. Haynes and appeared in his DA Materials Handling Equipment Primer series. Mr. Haynes' series is being expanded considerably and printed in book form. The book, "Materials Handling Equipment," will be published this year. The tentative price has been set at \$15. Inquiries should be directed to Mr. C. A. S. Heinle, Chilton Company, 56th and Chestnut Streets, Philadelphia 39, Pa.

The National Wooden Pallet Manufacturers Association has published several booklets which may help you. They are: "Pallets and Palletization," and "What You Should Know About Expendable Pallets." Inquiries should be directed to the Association at 910 17th Street, N. W., Washington 6, D. C., attention of Mr. William H. Sardo.

A "Pallet Repair Manual" is available at \$1 a copy from the Office of Technical Service, Department of Commerce, Washington 25, D. C.—The Editor.

Letters for publication in DISTRIBUTION AGE must be signed and must give the writer's title and company affiliation. Names will be withheld upon request.

DISTRIBUTION AGE



...increasing productivity without adding personnel!

Management men in every industry are giving productivity and profits—as well as *personnel*—a lift by integrating Towmotor mass-handling in all operations from receiving to delivery.

Every department benefits. More work gets done. And you don't need more men to do it because Towmotor fork lift truck operators are finding it's *fun* to multiply the handling of materials aided by 12 new Towmotor improvements like these:

- New functional body styling
- New pace-setting comfort features

- New "tight-spot" maneuverability
- New centralized easy-reach controls
- New positive-response hydraulic lift
- Towmotor-improved power steering

Some of the industry's most significant advances in fork lift truck design and performance are shown in Towmotor's latest bulletin #SP-23. See how

they help supplement your personnel—permit your men to do a bigger day's work easier—give each operator the power to improve your plant's productivity and profits.

Write Towmotor Corporation, Cleveland 10, Ohio. We'll send the illustrated booklet immediately.

**Leaders for 38 years
in building Fork Lift Trucks
and Tractors**



Gerlinger Carrier Company, Dallas, Oregon is a subsidiary of Towmotor Corporation, Cleveland, Ohio

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#1 air cargo service to all Europe

**NO WAITING! NO DELAY! 250,000 POUNDS OF LIFT WEEKLY
61 FLIGHTS WEEKLY PLUS 6 ALL-CARGO**

- *Most frequent transatlantic cargo service of all.*
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Free booklet. To make the best use of air cargo services, send for FREE booklet, "There's Profit in the Air." Call your PAA District Sales Manager or write to Pan American World Airways, P. O. Box 1790, New York 17, N. Y. For further information on routes and rates, call any Pan Am office or your own agent. In New York, call STillwell 6-7341. *Trade-Mark, Reg. U.S. Pat. Off.

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Flies more cargo overseas
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Istanbul
Keflavik
Lisbon
London
Munich
Nice
Oslo
Paris
Rome
Santa Maria
Shannon
Stockholm
Stuttgart
Vienna

What one form of power is cheaper today than in 1940?



Electricity. And if you operate electric industrial trucks, the cost is probably less now than when you bought them.

Isn't this a refreshing contrast to the rest of today's business-cost picture? And isn't this something to think about for the future? Other costs keep going up, up, up. But electricity has been coming down, down, down—for years.

The result is that electric industrial trucks are a better power bargain today than they've ever been. And power savings plus overhead savings actually total hundreds of dollars per truck every year. This has been proved in thousands of cases in all types of truck service.

Here's how you save: You buy the power from your local electric company at low rates. Heavy duty storage batteries last for years without replacement. Cool-running electric motor-drive systems that seldom need maintenance—use no power at all when standing still. While first cost is higher, electric industrial trucks give you more than twice the useful life, according to national average figures. And it's a fact that of all the electric industrial trucks ever made in the U.S., about 90% are still on the job! That's real proof of service.

These are important cost considerations for up-to-date management men today. In addition, electric industrial trucks are easier to operate and make practically no noise—advantages especially appreciated by plant personnel. Have a talk with your nearby industrial truck dealer or salesman. He's listed in your classified telephone directory under "Trucks—industrial."

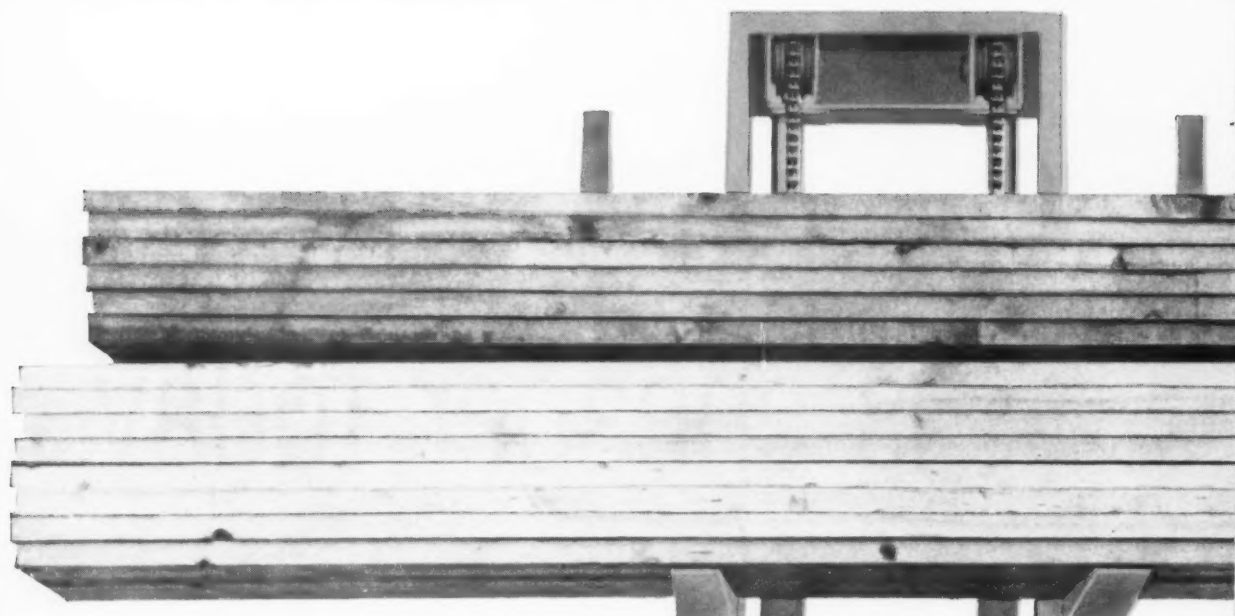
This message is presented as a service to industry by Exide Industrial Division, The Electric Storage Battery Company, Phila. 2, Pa.

THE ELECTRIC STORAGE BATTERY COMPANY **Exide[®]**

Circle No. 10 on Card, Facing Page 51, for more information

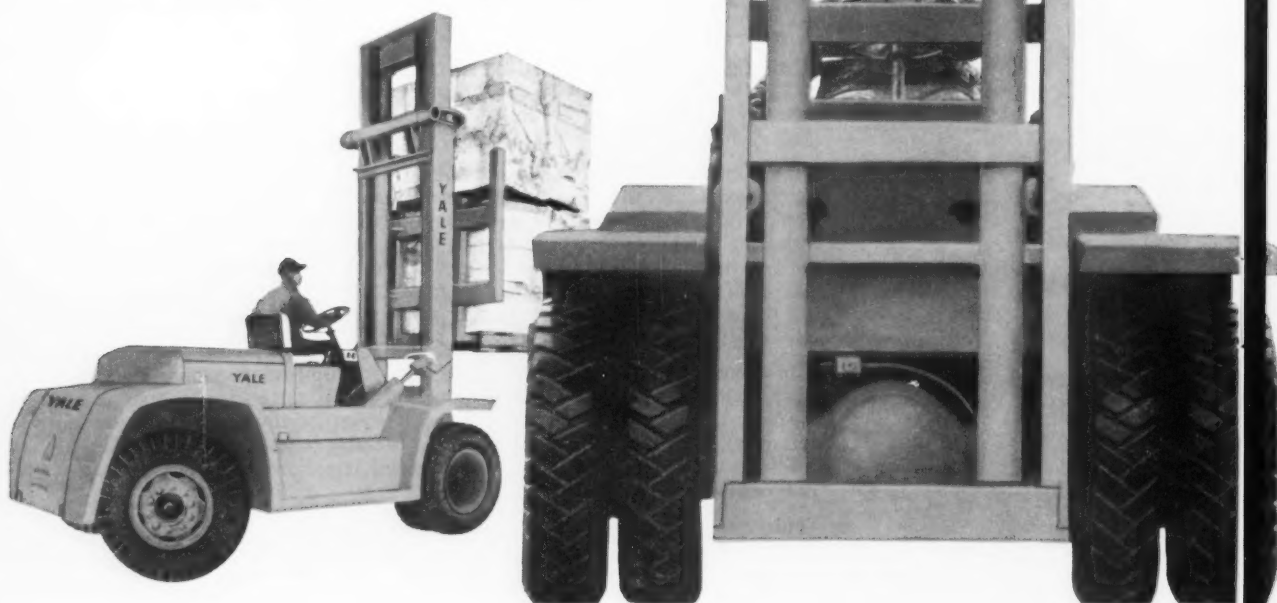
To meet the need of expanding industries for better materials handling methods, look to Yale for advances in research, engineering, manufacturing, sales and service—as YALE BUILDS FOR THE **NEW ERA**

Now...first and

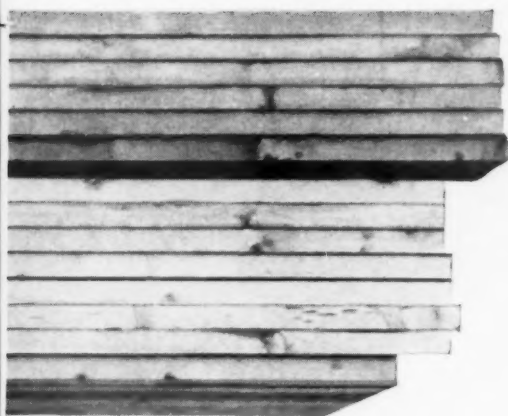


Easily Handles 15,000 to 20,000 lb. Loads

New Yale High-Capacity Gas Trucks handle extra-heavy loads with maximum speed, safety and efficiency—in both indoor and outdoor operations.



only high capacity gas truck with full visibility



YALE

WIDE-ANGLE VISION

300% MORE VISIBILITY THAN ANY OTHER TRUCK OF ITS TYPE!

Not just straight-ahead vision—but “corner of the eyes” vision, too—*fully functional* visibility with the big, open look of new Yale High-Capacity Gas Trucks. Operator sits high and in center. Upright channels are wide-spaced, nested to reduce frontal obstruction. Right next to them—the hoisting cylinders, one right, one left. Chains that raise carriage are in front of cylinders—*out of the arc of vision*.

TOP PERFORMANCE UNDER TOUGHEST CONDITIONS

Here are just a few of the premium engineering features—all included as *standard*—that give new Yale Gas Trucks a big edge in stamina and safety under all conditions:

- I-beam side members in frame for extra strength.
- Channels mounted on ball-bearing rollers to reduce friction and wear.
- Ball-mounted, self-aligning hoisting cylinders.
- Gear shift lever, plus hoist, tilt and attachment lever at right side of driver. Directional control lever conveniently located on steering column.
- Power steer with two balanced hydraulic cylinders, one to each rear wheel, to reduce stress on bearing points and linkage.
- Your choice of Yale Fluid Coupling or standard transmission.

Carriage has one top bar, one bottom bar—good visibility between them at all levels of fork lift. Channel cross members are at channel ends—none between. When an attachment is used, hydraulic hoses are located in front and behind cylinders—not in the line of sight. Whether looking forward or to the rear, the operator sees more. Result: these new Yale trucks with “Wide-Angle Vision” promote safer, faster, more efficient handling of 15,000 to 20,000 lb. loads.

GREATER STABILITY. Lifting, lowering, traveling—these Yale trucks keep capacity loads in precision balance! Minimum weaving with full loads, because the trucks are built low—low center of gravity, with high underclearance. Extremely rugged design. Broad lifting base afforded by two ball-mounted lift cylinders. Wide channel-roller spacing at all points of lift, plus side-thrust rollers on channels and carriage, further increase stability factor. Large-size, high-flotation tires give good traction under severest terrain conditions.

FAST CYCLE OPERATION. New Yale High-Capacity Gas Trucks have lifting speeds up to 60 feet per minute; fast, safe lowering; travel speeds up to 20 miles per hour; easy maneuverability.

Designed for complete selection of attachments—engineered for a minimum loss distance.

YALE*

*REG. U. S. PAT. OFF.

INDUSTRIAL LIFT TRUCKS AND HOISTS

YALE & TOWNE

The Yale & Towne Mfg. Co., Materials Handling Div. Manufacturing Plants in Philadelphia, Pa., and San Leandro, California

The **YALE & TOWNE** Manufacturing Co., Dept. A-25
Roosevelt Blvd., Philadelphia 15, Pa.

Please send the new Yale High-Capacity Gas Truck
brochure No. 5230.

Name _____ Title _____

Company _____

Address _____

City _____ Zone _____ State _____

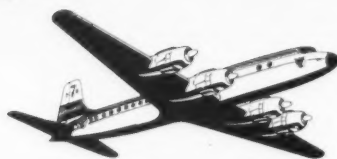
*In Canada: write The Yale & Towne Manufacturing Co.,
St. Catharines, Ontario, Canada*

Gasoline, Electric & LP-Gas Industrial Lift Trucks • Worksavers • Warehouseurs • Hand Trucks • Hand and Electric Hoists

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HERE'S A NEW AIRFREIGHT SERVICE

365 M.P.H. FAST



between Chicago • Denver • Los Angeles

(Kansas City service starts in July)

**CONTINENTAL
AIR LINES
NEW
ALL DC-7B
AIR FREIGHT
SERVICE**

**COMBINING ALL THE MOST WANTED
FEATURES OF AIR FREIGHT
SHIPPING INTO ONE NEW SERVICE!**

DC-7Bs on Every Flight:

With the speed of DC-7Bs, you can deliver your product in a hurry...when the buyer wants it...when it's moving! That means faster turnover...more efficient use of capital!

Overnight or Same-Day Delivery:

Delivery-wise, you can be competitive in cross-country markets. And, Continental Air Freight puts your product on a prestige pedestal...makes it more important to customers and dealers.

Free Reserved Space:

"Reserved Space" on Continental Air Freight assures perfect timing in shipping. Helps you reduce inventories by planning production to coincide with shipping. Cuts costly warehousing!

Individual Attention:

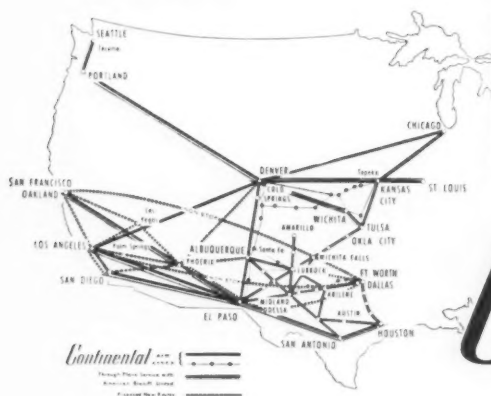
The "kid glove" treatment accorded all Continental Air Freight shipments reduces the need of costly packaging. Reduces tare weight!

One-Carrier Service to Major Cities:

Continental's DC-7B Air Freight service makes fast Continental connections to most of the major West and Southwest markets for faster, more efficient delivery.

*Call your nearest airfreight forwarder,
or Continental Air Lines.*

**CHICAGO—LUdlow 5-6800
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LOS ANGELES—OREgon 8-3943**



Continental **AIR LINES**



By Walter C. Pine

*Traffic Manager
The DeLaval Separator Co.
Poughkeepsie, N. Y.*



Traffic Management and Integration

A SUCCESSFUL traffic department of a completely *integrated* company—a company that manufactures, sells, and distributes its own products—is one that *integrates* itself with all other departments of the company.

In daily contact with other departments, Traffic is in an excellent position to affect cost savings for those departments. While Traffic's principal area of influence is in shipping cost management, we have found that our cooperation with other departments produces additional savings.

Although our program in connection with the Sales Department is described in some detail below, we do not limit integration to just a few of the firm's major departments. We work closely with all departments. A working arrangement similar to the one outlined here for the Sales Department exists for every other department in the company.

Purchasing

There is an obvious tie between Traffic and Purchasing. We advise Purchasing on the most advantageous purchasing areas from a transfer standpoint. We also provide information on the most economical size of a particular purchase, and attempt to consolidate its shipments.

The Traffic Department assists Purchasing in the preparation of contracts where terms affect traffic. Our information on rates, routes, and classifications for potential sources of supply enables the Purchasing Department to

In the interest of economical distribution of goods, the successful traffic department must integrate itself with all the other departments

compute comparative costs. We also keep Purchasing informed on impending changes in rates and routes.

Production

Although the tie between Traffic and Production may not seem so obvious, we have been able to work out a beneficial program. Among other things, Traffic advises Production on materials handling methods, packing and packaging specifications, and disposal of scrap metal and other waste products.

We help production maintain a steady flow of two-way traffic for raw materials and finished products. We also help the Production Department meet schedules by providing an adequate supply of carrier equipment for safe and quick delivery of incoming materials.

Advertising

Our connection with the Advertising Department chiefly concerns shipping procedures. We control shipments of outgoing advertising matter in accordance with planned campaigns. This material includes literature, samples, displays, and

booth equipment for trade shows. We also advise Advertising on descriptive matter on packages in respect to classification.

Packaging & Export

Careful evaluation of the individual container and study of the particular problems of the product for which it is designed are vital. Proper container design in accordance with classification rules also is important. We are able to assist the Packaging Department in these matters, as well as in reducing the weight factor for lower freight costs, and determining the most economical unit container.

All of these functions also are performed for the Export Department, in addition to the preparation of proper documents, letters of credit fees, and marking requirements.

Legal & Accounting

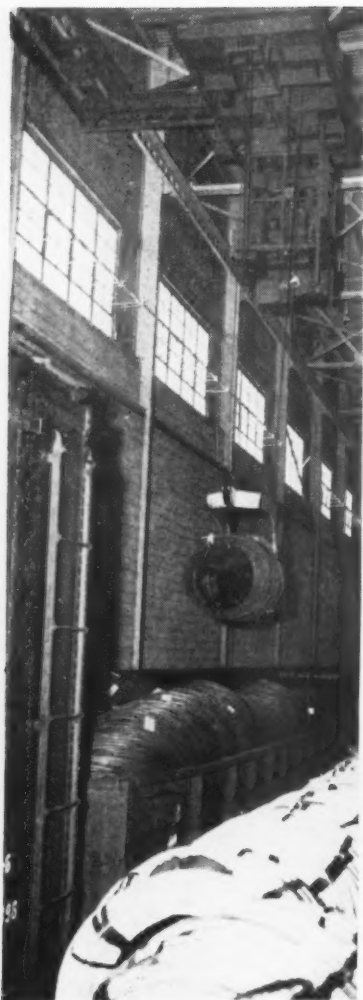
For the Legal Department, Traffic prepares rate case and loss and damage claims evidence. We assist the Accounting Department by auditing freight bills, furnishing proof of delivery for the Credit
(Please Turn to Page 76)

Overhead Handling Creates New Approach to Storage

Vertical storage crib for wire, specially-designed waste conveyor, and conveyor system to supply empty kegs are innovations in new Ohio plant

By Robert Widmer

Traffic Manager, The Cleveland Cap Screw Co.



ARE-APPRAISAL of materials handling equipment as part of a plant relocation program has resulted in several innovations at our new plant in Cleveland.

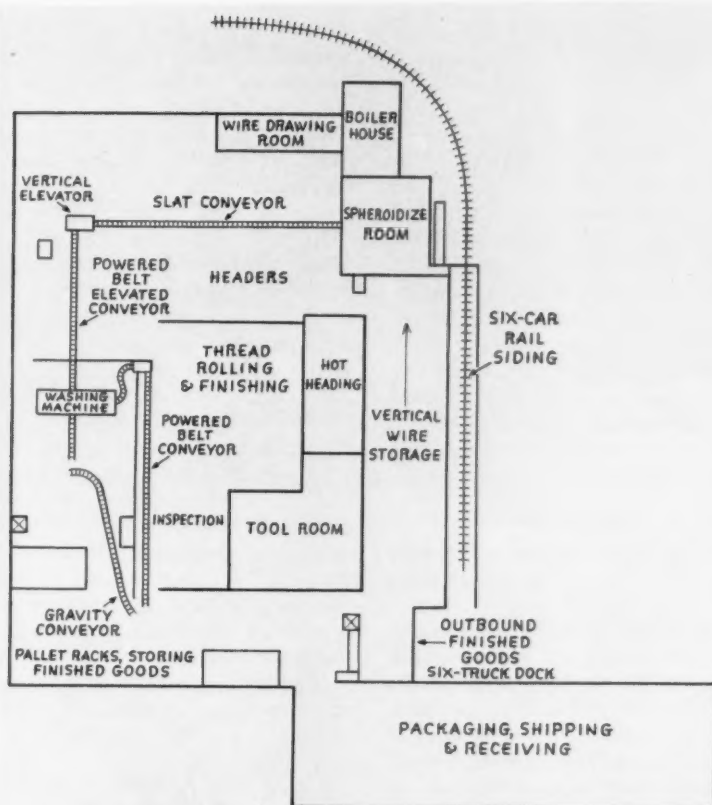
Designed to make traffic operation easier in years to come, the new equipment includes a vertical storage crib for coiled wire, a specially-designed waste conveyor, and a modern keg conveyor system. They mean faster shipments and increased production.

As one of the largest cap screw manufacturers, the company recently completed a move from a small, overcrowded plant to a new \$5,000,000 structure in southeast Cleveland.

Located in our new plant is a revolutionary type of storage arrangement for coiled wire. It is a vertical storage rack made up of a series of hollow steel pipes joined together in a geometric pattern by means of a series of triangular steel plates. Pointing

Electrically-powered crane (left) carries coil of wire from railroad car to vertical storage crib (below). Alligator jaws expand to hold coil





Left: Wire for bolt making arrives by rail (right) and is stored in vertical storage cribs. From there, raw materials move to production area (top of the plan). They then are carried on slat and powered belt conveyors to a washing machine. One conveyor carries some bolts to heat treatment. Others go directly to finishing. The scrap conveyor runs parallel to the slat conveyor through bolt making area. Scrap collects at the left. A keg supply conveyor originates at left of plan. It ends in areas where kegs are used

they are pushed apart and forced against the inside of the coil.

Held by intense internal pressure, the coils then are lifted from gondola cars at the side of the receiving dock on a spur track, swung over the storage area and deposited into a vacant nest. When the wire is needed for manufacturing, the crane lifts the coil and carries it to the Wire Cleaning Department.

The spur, branched into the main freight line of the Erie Railroad, permits as many as six freight cars to enter the plant.

Scrap Removal

The problem of removing scrap metal from around machines and out of the plant usually is a slow and costly operation. At Cleveland Cap Screw, steel chips and fragments accumulated when cap screws are extruded in boltmakers are removed mechanically. The scrap is handled by a special chip conveyor—an endless bucket-type conveyor belt located in a wide,

(Please Turn Page)

upward like giant fingers, the posts form a great many separate nests for the storage of coiled wire.

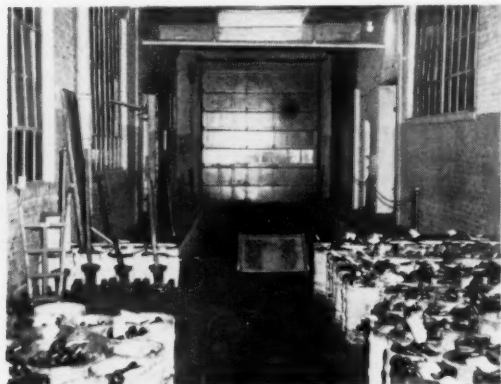
About 18,000 to 22,000 lb of hot rolled steel wire in coiled form can be stored in each triangular nest between the 16-ft posts. Weight of individual coils range from 600-1800 lb.

The vertical storage crib is lo-

cated on the receiving dock in a covered area which measures 98 x 400 ft. It saves an estimated total of 40,000 sq ft of floor space.

An overhead, electric crane, a part of the storage arrangement, is equipped with a special lifting device known as "alligator jaws." Hydraulically-actuated, the jaws move laterally. When inserted into the center of a coil of wire,

BEFORE—A tight squeeze awaited trucks at one of the two docks in old plant. Dock areas were crowded



AFTER—New shipping and receiving department has room for six trucks. Trucks use a separate driveway



Overhead Handling . . .

(Continued from Preceding Page)

concrete ditch below floor level. This concrete fissure runs the full length of the manufacturing department between rows of bolt-makers, machines that produce cap screws. Pneumatic, gas and water lines also are located in the ditch. All are covered by one-half inch steel plates.

As the oily chips and fragments of steel fall away from the punches and dies, the scrap falls into tubes—one to a boltmaker—and drops down into the bucket conveyor. The line moves out to the edge of the plant and into a hopper where oil is removed. The scrap then is deposited into a bucket-type container. Later it is hauled to waiting gondola cars for shipment to scrap dealers.

Keg Storage Line

Another innovation is an overhead keg storage conveyor line composed of a series of two-keg cradles which move empty kegs from a receiving area to the manufacturing area. These cradles carry empty kegs toward, and through the manufacturing area at the rate of 120 fpm. It is estimated that manual handling of finished goods will be reduced approximately 25 per cent as a result of the keg conveyor installation. Utilization of space overhead

keeps aisles clear between the boltmakers, and the sturdy rails and cradles permit safe movement of kegs, both filled and empty, in a continuous cycle.

Powered belt conveyors, rollers, small electric hoists for lifting tote boxes in the Heat Treating Department, were used at the former plant. All of these have been incorporated successfully into the materials handling system at the new Cleveland Cap Screw plant.

Storage Space

A serious shortage of storage space experienced at the old plant has been relieved.

The old plant was greatly handicapped by lack of space, and it was difficult to keep large stocks of standard cap screws on hand. The new plant has 30,000 sq ft of bulk storage. This is enough space to keep 40,000 kegs of finished cap screws on hand at all times.

Kegs are palletized in bulk storage at the new plant. In the production of special-headed products, finished goods sometimes go directly to the shipping docks for immediate delivery to a customer. Standard sized cap screws, on the other hand, go into bulk storage to fill orders as they are received from sales representatives or customers.

In the bulk storage area, steel racks are used for the stacking of palletized kegs. Racks are high enough to handle palletized kegs six high. Enough room was provided between aisles to permit fork lift pallet stacker trucks equipped with special arms to reach all racks without difficulty. Also the trucks are able to turn easily in the aisles without scraping sides of racks.

An inventory record of each rack section is mounted on main aisles so that stock on hand can be checked quickly and changes made as kegs are removed for shipment.

A special truck drive from busy Lee Road which passes the front of our plant leads directly to shipping and receiving. It is wide enough for several trucks to pass abreast, permitting tractor-trailers to go and come with a minimum of delay.

The plant is built of steel frame construction with brick veneer and corrugated siding exterior. Floors and supporting columns are built of reinforced concrete, covering a total area of 400,000 sq ft.

Making the Move

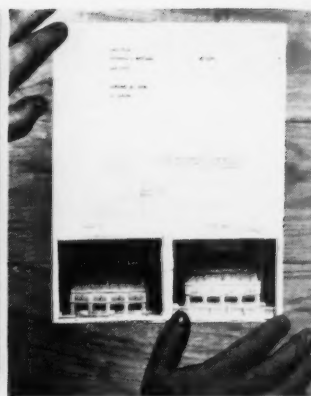
One of our biggest problems in moving from an old plant to a new one was the transport of heavy machinery. Over a period of several months, we systematically transferred equipment from our 35-year-old plant on E. 79th Street into the new plant. All heavy machinery
(Please Turn to Page 73)

Power belt and roller gravity conveyor line moves kegs from the Heat Treatment Department to Bulk Storage



A section of the 30,000 sq ft bulk storage area. Storage racks will permit storing of 40,000 kegs in the new plant





Shipping Department employee photographs each interior end of the loaded rail car with a one-minute camera

Photographs showing condition of rail shipment are tacked (left) to inside wall of car, along with the packing list

A NEW customer service, which utilizes photographs taken of orders just prior to their shipment by rail, has been developed by our Shipping Department. Although it was developed principally as a customer service, the system has provided several side, or bonus benefits.

Pictures are taken with a one-minute camera of each interior end of the car. Two of the photographs are sent to the district sales office in the customer's area. Two more are placed with the packing list, which is tacked to the inside wall of the rail car itself, and two more are kept on file at the Newark office.

The photographs not only graphically show the customer the condition of his shipment when it left Newark, but also have been used by the customers as receiving reports.

Customer Reaction

Customer reaction to the system has been excellent. One customer has reported that the pictures inspired him to start a photographic record of his own in-plant materials handling techniques for damage control purposes.

We also have used the photographs in claims settlement. In some instances they have reduced the time involved. Travel time and expense involved in claims settlement also has been reduced.

Loading and bracing techniques

Customer Service via Photography

Photographs of outgoing rail shipments also expedite claims settlement, cut loss and damage ratio, improve loading and bracing techniques, serve as training aid

**By L. J. Meier, Production Superintendent
Kaiser Aluminum & Chemical Corp., Newark, Ohio, Works**

also have been improved through the use of photographs. In one instance a new technique was developed for loading re-draw rod. More steel strapping and less dunnage are used. The new system has resulted in faster rod loading and unloading time.

Loss and Damage Ratio

Photography has contributed to a lower loss and damage ratio. The lower ratio came about through a combination of improved packing techniques and improved quality consciousness on the part of loading crews.

We have found the photographs extremely helpful as training aids in departmental and company-wide training programs. Enlarged pictures are used to demonstrate correct and incorrect loading and bracing techniques. Training sessions have become more interesting to the employees and more valuable through the use of photographs.

Finally, the photographs offer an extremely valuable, but intangible benefit. They provide shipping supervision an easy, pictorial method of reviewing all outgoing rail shipments. •

By Carl Abell

JOHNS - MANVILLE Corp., world's largest manufacturer of asbestos products, has become one of the country's largest users of propane power for industrial trucks.

The company's Waukegan, Ill., plant has been operating an LPG fleet for some time. Another main plant, at Manville, N. J., now is almost entirely LPG equipped. The company's Watson plant, near Long Beach, Calif., recently converted its entire fleet. Test vehicles are operating at J-M's big diatomaceous earth quarries near Lompoc, Calif.

Results at all locations have been favorable. Lower fuel and other operating costs are reported, along with considerably lower maintenance and replacement costs.

Local Control

All of the conversions were handled independently by local plant management. A good interchange of information exists throughout the company, but the decision to make such changes, plus selection of equipment, is dependent upon local conditions.

This resulted in a diversity of conversion systems, but results have been uniformly good. Local managers study the field and select the system they feel will be most satisfactory. In all cases, availability of competent local service organizations has been an important factor. These organizations help with the installation and subsequent service operations.

The Waukegan location has about 45 acres under roof on a 260-acre tract. It consists of 12 manufacturing buildings, and warehouses and office facilities. In addition, there are extensive outside storage areas, dumps, and scrap yards. The outdoor areas are connected by several miles of private roads.

Initial Experiment

The first LPG conversion unit was placed in operation at Waukegan on an experimental basis



New trucks are purchased with standard gasoline engines and engine accessories, "broken in" on gasoline operation, then converted to LPG

This manufacturer of asbestos products is well on its way to converting its entire industrial truck fleet at four separate locations to propane power. Results so far have been excellent, with savings in fuel costs, better truck performance, and reduced maintenance costs

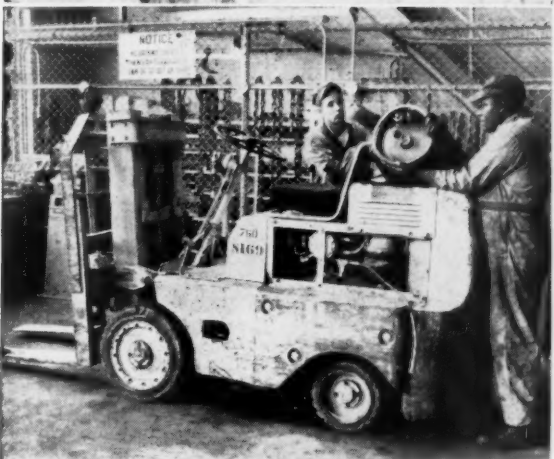
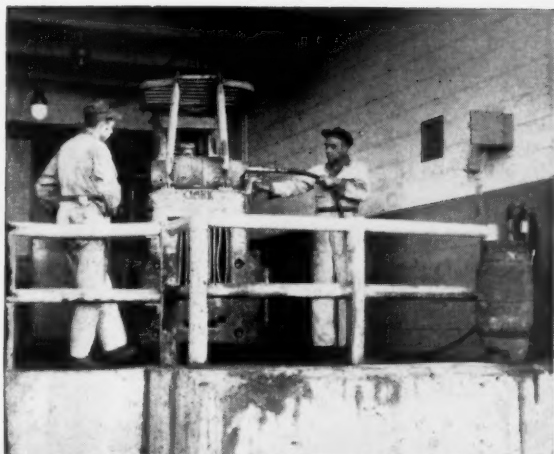
Switch to LPG Cuts

in June of 1953. A 7000-lb fork truck was selected and converted with a vaporizing unit. The truck was selected because of its state of excellent repair and the heavy work to which it was subjected.

The unit was placed in operation in the Asbestos Shingle Department of the Building Products Division. This particular job had had a record of extremely high fork truck maintenance.

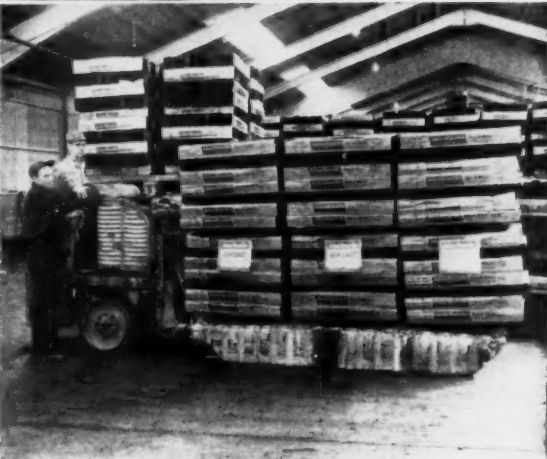
The test was continued over a

period of approximately one year. Analysis of the truck was incorporated in an appropriation request to convert all of the Building Products Division units to use propane. On the basis of the report, the appropriation was approved and the entire fleet was converted. These first units were converted gasoline engines in various states of repair, ranging from near salvage stage to new units.



Top: For safety purposes, indoor industrial trucks are brought to open dock for fueling operation

Bottom: First trucks converted at Waukegan had removable fuel tanks, now permanent tanks are used



Top: Experiment with industrial trucks was so successful that over-the-road equipment was converted

Bottom: Results indicate that fork truck maintenance costs, even on heavy trucks, will be cut one-third

Operation, Maintenance Costs

The conversions were of the spud-in carburetor type using vaporizers and demountable tanks. Installation of the conversion units was done under the direct supervision of the truck shop foreman, and a representative of the vendor supplying the kits.

As a result of the initial conversion, which proved quite satisfactory, other operating divisions at Waukegan and at other J-M locations immediately started

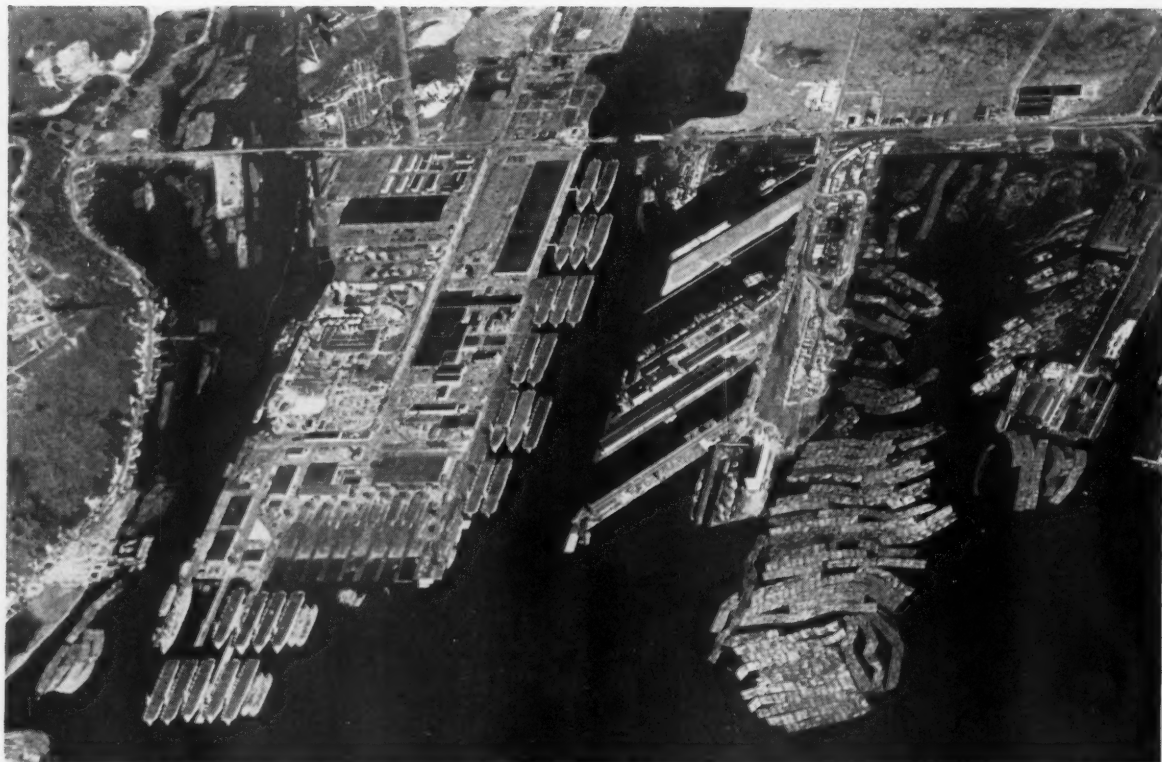
preparation for converting their units to use propane.

As experience was gained, it was found that better fuel-air ratios could be obtained by using a straight propane carburetor replacement and vaporizers. Subsequent conversions at Waukegan utilized these two components. All types of indoor and outdoor industrial trucks were included in the conversion program, ranging in capacity from 3,000 to 18,000

lb. At least eight different truck manufacturers were represented.

Engine Condition

The customary practice in converting a fleet is to put the propane equipment on engines that are known to be in good condition. Those in need of rebuilding generally are overhauled before the conversion is made. On the strength of information gained
(Please Turn to Page 71)



By Warren E. Crane
DA Pacific Northwest Correspondent

Port of Tacoma

TAKING advantage of its fine natural facilities, the Port of Tacoma has undertaken an ambitious port development program. The program was designed to attract more industry to the Port's Industrial District.

In claiming Tacoma as one of the world's five best natural harbors, Port officials point to Commencement Bay's excellent physical facilities. Tacoma's harbor from the Pacific Ocean is through the Strait of Juan de Fuca, to Pudget Sound. The Sound is a wide passage with deep water and no obstructions.

Commencement Bay itself is four and one-half miles wide at its entrance. It has an average width of nearly two miles and is two and one-half miles long. Average depth is 180 ft, and there are no bars or other obstructions.

At the head of the harbor lies the Port of Tacoma Industrial District. The District consists of some 2000 acres broken into industrial sites. Sites are available with and without deep water frontage. The District is served by city utilities and is administered by the Port Commission.

Efficient dispatch of cargo is the prime concern of the Port. Modern installations are available to

Port development program aims

expedite the handling of all types of cargo.

In addition to its marine terminal facilities, the Port also operates a modern four-story facility for cold storage and sharp freezing. This unit has a capacity of two million cubic feet. Adjacent buildings house facilities for the processing of vegetables and fruit for sharp freeze. Products are handled through four freezing tunnels in the main plant.

The Port has a two-million bushel capacity grain elevator. The workhouse is provided with three receiving legs with space reserved for two additional legs possessing a capacity of 100 cars a day. Two shipping legs provide ship delivery at a maximum rate of 30,000 bushels an hour. Rail facilities provide for 50 cars on spot at the unloading track. There also is a 50-ton hydraulic automatic grain truck dumper. Depth at dockside is 35 ft at low tide.



The Port of Tacoma is called one of the world's five best natural harbors. Commencement Bay is four and one-half miles wide at its entrance, has an average width of two miles, and an average depth of 180 ft with no obstructions

Woos Industry

at expansion of industrial district

Pier No. 1 is an open dock 166 ft wide and 1,200 ft long. It has three surface tracks on the face of the pier. Handling facilities include two 7½-ton movable electric hammerhead cranes, each with a 90-ft reach, and one 20-ton steam locomotive crane with a 50-ft reach.

There is berthage for two ocean-going vessels in the slip and one on the face of the pier. Storage space for 15 million board feet of lumber is available. Additional space on open ground adjacent to the pier has a capacity of an additional 25 million board feet.

The open ground area also is served by a 7½-ton movable electric hammerhead crane. There is a heavy lift crane on the face of the pier of 60-ton capacity at a 60-ft radius with a full 360-deg swing.

Pier No. 2 is 180 ft wide and 1,040 ft long with

WATERBORNE COMMERCE

Year	Arrivals	Tonnage	Departures	Tonnage
1930	2028	6,026,872	2021	5,965,400
1935	1874	4,661,168	1872	4,648,085
1940	1230	3,302,027	1236	3,329,374
1950	828	2,773,556	828	2,773,556
1952	973	2,944,782	962	2,900,782
1953	875	2,837,456	875	2,837,456
1954	1757	3,331,969	1749	3,342,143
1955	*2676	3,621,823	*2685	3,642,027

*Includes barge movements not previously reported.

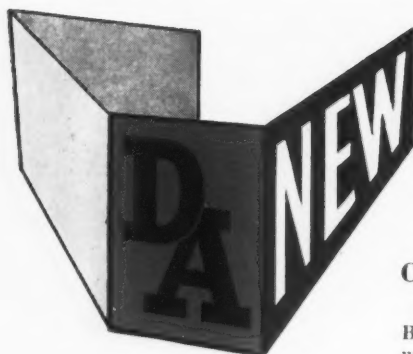
IMPORTS and EXPORTS

Year	TONS		DOLLARS	
	Imports	Exports	Imports	Exports
1930	3,117,545	1,841,744	\$ 71,593,612	\$ 73,871,386
1935	1,012,559	1,503,987	59,087,523	48,302,664
1940	1,486,529	1,140,347	58,206,267	65,868,854
1950	2,401,172	745,588	109,075,680	91,707,324
1952	2,229,363	1,327,714	106,843,534	149,923,386
1953	2,208,577	804,515	110,379,189	109,114,967
1954	3,127,085	1,130,992	133,189,761	135,050,035
1955	2,707,997	1,659,082	134,413,331	161,549,101

a 180,000-sq ft storage shed. It has a 60,000-sq ft apron for direct transfer of cargo between vessels and open cars.

The apron is served by two five-ton electric semi-portal cranes. The facility also includes a loading platform at the rear of the shed, 12 ft wide, paral-

(Please Turn to Page 74)



PRODUCTS

... FOR FURTHER INFORMATION

Light-Duty Fork Truck

Shorter turning radius and other extra maneuverability may be gained with a new series of light-duty electric fork trucks introduced by Baker-Raulang Co. Two models are now



available: FTA-50, with 5000 lb capacity, and FTA-70, with 7000 lb capacity. Specifications of the FTA-50 include 77 $\frac{1}{4}$ -in. turning radius, 68 $\frac{1}{4}$ -in. minimum intersecting aisle, 41-in. overall width, and overall length without forks of 79 $\frac{1}{4}$ in. Turning radius of the FTA-70 is 86 $\frac{1}{4}$ -in. Minimum intersecting aisle is 74 $\frac{1}{4}$ -in. Overall length without forks is 89 $\frac{1}{4}$ -in., and overall width is 44 $\frac{1}{2}$ -in.

Circle 30 on Card Facing Page 51

Pressure-Sensitive Labels

Paramount Paper Products Co. has available over 100 new die-cut pressure-sensitive label designs. The line includes varying sizes and types of price and sales labels, bill collecting labels, shipping labels, etc. They are produced in flat sheet and roll form depending on design and intended use. All are printed on a sparkle finish paper.

Circle 31 on Card Facing Page 51

Cargo Trailer

The new lightweight trailer of Brown Trailers, Inc., when coupled with its exclusive air-ride running gear, weighs only 7920 lb complete with 10-20 tires, making possible an additional 1700 lb of payload. Among

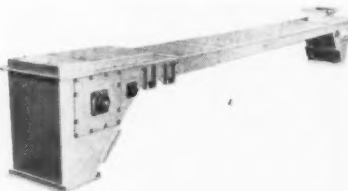


the weight-saving factors are aluminum slat lining, and extruded aluminum flooring instead of the conventional quarter-inch fir plywood. Another advantage is the elimination of spring deflection, permitting constant, lower floor level and an increased body-loading height.

Circle 32 on Card Facing Page 51

Slider Board Belt Conveyors

Sprout, Waldron & Co., Inc., is producing a new series of slider board belt conveyors suitable for conveying fragile materials over horizontal distances of up to 300 ft. It has low power requirements, large capacities, and will handle travel up and down slopes. This new development will



handle most bulk materials which do not contain large lumps. It is particularly desirable for handling materials which other types of conveyor would fracture. It consists of an endless belt operating on a smooth sheet metal trough. It is enclosed between skirt plates extending the full length of the conveyor.

Circle 33 on Card Facing Page 51

Off-Highway Truck

Dodge Truck introduces the smallest of its new four-wheel drive Power Wagons, the W100, offered with either a 204-hp V-8 engine or a 120-hp 6-cylinder engine and in 108 and 116-in. wheelbases. The new trucks can climb 60 per cent grades with loads and are designed chiefly for off-the-highway uses and for travel through sand, mud, and snow. The W100 has



a maximum vehicle weight rating of 5100 lb. Capacity of the power winch available on this model is 8000 lb. A wide variety of body styles and many options in transmission, springs, tires and other component parts are available. Power take-off assemblies are available in all models for auxiliary power equipment.

Circle 34 on Card Facing Page 51

Yard Ramp

A portable, all magnesium, yard ramp has been introduced by Lite-Line Metal Industries Div. of Copperloy Corp. It permits freight car and truck loading where permanent dock facilities are not available. Strong, lightweight bridge type girder construction with extra deep side beams assures minimum deflection under load. Safety tread surfaces assure all weather trac-



tion, and safety curbing along the full length of the ramp prevents vehicle run-offs. A tow bar is provided to allow fork truck hauling.

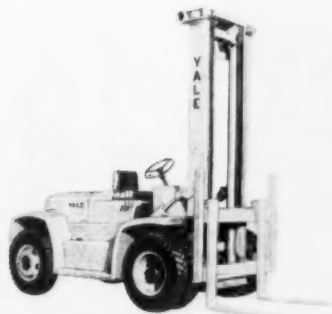
Circle 35 on Card Facing Page 51

and EQUIPMENT

PLEASE USE THE READERS' SERVICE CARD • • PAGE 51

Powered Lift Truck

A new line of gasoline powered, pneumatic tire lift trucks has been announced by **The Yale & Towne Mfg. Co.** Among the new features of the G-3 line are the channel construction providing 300 per cent more visibility for the operator, greater load stability

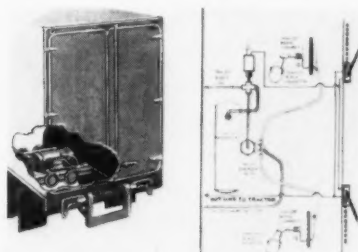


and the fastest possible cyclic rate of operation within the limits of safety. The line is being introduced in 15,000-lb, 16,500-lb, 18,000-lb and 20,000-lb capacity models powered with gasoline or LP-gas and utilizing either a straight, friction clutch transmission, or fluid coupling transmission.

Circle 36 on Card Facing Page 51

Automatic Brake Control

Dockett, offered by **Dockett Mfg. Co. Inc.**, incorporates two aluminum arms attached to the rear end of the trailer. When depressed by contact with dock, the arms activate a switch which sends current to a valve assem-



bly, allowing air to flow to brake boosters. Adaptable to all air brakes, it also prevents damage to docks and dock doors. It eliminates cargo damage caused by the sudden, jolting impact when trailer hits dock. It also provides emergency braking power on the road if regular brake line fails.

Circle 37 on Card Facing Page 51

Warehouse Hand Truck

A new 1500-lb capacity hand truck specifically designed for manual handling has been announced by **Mercury Mfg. Co.** The truck can be equipped with assemblies for either under-floor or overhead conveyor line operation. Platform size of the Type A-308 hand



truck is 36 in. wide by 60 in. long and it stands 14 in. high. It has a flush hardwood deck with countersunk metal holddown strips. The unit has two stake pockets on 23-in. centers on the caster end to accept a pipe rack, with one crossmember, for pushing.

Circle 38 on Card Facing Page 51

Dial Radiotelephone

The first completely automatic dial-direct mobile two-way radiotelephone system has been made available by **Allen B. DuMont Laboratories, Inc.** This system allows phone calls to-and-from vehicles to be relayed, completely unattended, through local telephone systems. Calls are made by simply dialing a number within a local telephone system; also, anyone within that system can call the vehicle in



the same manner as when making a house-to-house call. Only two telephone lines are required to interconnect the base station equipment with the telephone company's central office.

Circle 39 on Card Facing Page 51

Postage Meter

A new improved Post-O-Meter, manufactured by **Detecto Scales, Inc.**, is designed to eliminate expensive de-

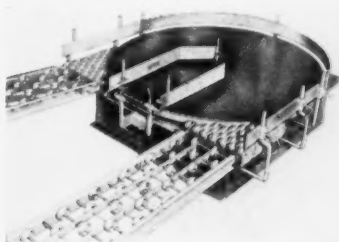


lays caused by underpostage and extra costs of over-postage. The parcel is placed on the platform and the correct zone selected by pressing the proper key. A hair-line indicator automatically gives the exact rate for each zone. Capacity is 70 lb x 2 oz and computes postal rates in all zone classifications.

Circle 40 on Card Facing Page 51

Powered Turntable

Samuel Olson Mfg. Co., offers a powered turntable designed for use with powered or gravity conveyors. Available with adjustable legs to bring the top to any practical height, it can transfer packages or loads at either a 90 or 180-deg. turn. The 48-in. diameter disc is direct-driven by a gearmotor at 25 rpm to produce approximately 300 fpm rotation. The



disc is solidly supported by four rubber-tired ball bearing wheels which insure an even top surface under heavy loads.

Circle 41 on Card Facing Page 51

DA NEW PRODUCTS and EQUIPMENT

CONTINUED FROM PREVIOUS PAGE

Heavy-Duty Truck

New heavy-duty six-wheel truck designed for off-highway operation has been introduced by the motor truck division of **International Harvester**



Co. GVW rating is 60,000 lb. Standard equipment includes a 15,000-lb front axle, 46,000-lb rear bogie, hydraulic full-power steering, air brakes, 12-volt electrical system, 15-in. clutch, 60-gal right-side step fuel tank and heavy 2 $\frac{3}{4}$ -in. steel grille guard.

Circle 42 on Card Facing Page 51

Fire Extinguishers

Portable dry chemical fire extinguishers, the smallest now available bearing approval of the Underwriters'



Laboratories, are offered by **Leeder Mfg. Co.** Available in 1-lb, 1 $\frac{1}{2}$ -lb, 2 $\frac{3}{4}$ -lb, and 4-lb sizes, they are said to develop maximum fire extinguishing power in minimum size containers.

Circle 43 on Card Facing Page 51

Storage Racks

A new method of materials handling and bulk storage has been devised in drive-in racks by **Burlington Fabricators, Inc.** The principle of operation is that of a multiple row rack

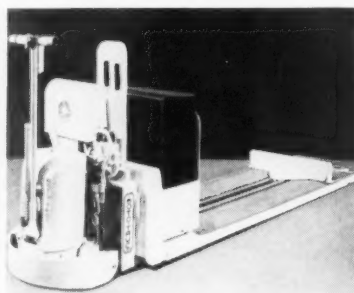


which permits penetration by the lift truck to the full depth of the rack, where the loaded pallet is set down. There is no limit to height of storage, other than that placed by ceiling and lifting equipment. Installed with a wrench according to the individual plan of application, the drive-in rack supports itself.

Circle 44 on Card Facing Page 51

Walkie Platform Truck

A walkie type platform truck equipped with a pusher bar for self-unloading has been developed by **The Moto-Truc Co.** The new truck is designed to permit loading of freight cars and highway trailers with long flat loads



such as steel sheet, plywood, building materials, etc. Dimensions of the standard truck are 135 in. long x 39 in. wide. Lowered height of the platform is 2 $\frac{5}{8}$ in. with the forward end tapered to 1 in. to facilitate pushing loads onto the floor.

Circle 45 on Card Facing Page 51

All-Hydraulic Fork Truck

An all-hydraulic line of gasoline or LP-gas operated fork trucks, known as **Speedlift**, is available from **Pettibone Mulliken Co.** This new design eliminates clutch, transmission, differential, drive shaft, and differential axle. For transmission of power, an engine turns a hydraulic pump which actuates a hydraulic motor. Weight of the truck is 4380 lb, making it suited for operation in areas where floor ca-



pacities are limited. It will spin and turn in its own length (overall length less forks, 84 in.) as rear wheel turns a full 90 deg. in either direction.

Circle 46 on Card Facing Page 51

Stationary Lift

Langley Mfg. Co. announces its platform type, self contained, stationary **PowrLift**. Operating from ordinary lighting circuit lines in 500, 1000, and 2000-lb capacities, available up to 8000 lb. Basic platform sizes are 24 x 24 in., 30 x 30 in., 42 x 42 in., and 48 x 48 in. Basic platform raised heights are 60, 72 and 96 in. with one piece masts 120, 144, 168 and 192 in.

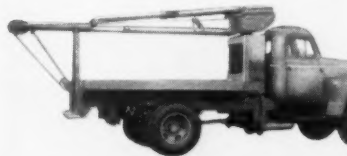


with spliced masts for easy shipment and erection. The platform down height is 3 $\frac{1}{2}$ in. so that installation in a 4-in. deep pit will allow the platform to come flush with the floor.

Circle 47 on Card Facing Page 51

Truck Crane

Requiring only 18 in. of space behind the truck or tractor cab, the Truck Crane, manufactured by Anthony Co.,



leaves the body free for useful payload area. This crane is built into the truck which permits loading, hauling and unloading with one work-unit. Power is completely hydraulic in all phases of operation with PTO driven pump. Boom swing is 280 deg. It projects or retracts in horizontal position or at any point up to an 85 deg. elevation. Capacity is 5000 lb.

Circle 48 on Card Facing Page 51

4-Wheel Drive Trucks

Chevrolet Motor Div. is in production of 12 models of four-wheel drive trucks. The new four-wheel drive vehicles will meet the demands of specialized truck users by supplying additional power and traction demanded by many off-highway operations. Available in the Suburban Carryall; the half, three-quarter and one-ton pickup; one-ton panel; and



three-quarter and one-ton stake truck models. The power is supplied through a four-speed transmission, then split between the front and rear wheels through a two-speed transfer case. Front wheel drive may be engaged or disengaged at any time without using the clutch, provided the transfer case is shifted into direct drive.

Circle 49 on Card Facing Page 51

Labeling Tapes

A new line of self-sticking paper label stocks has been announced by Permacel Tape Corp. It consists of 33 different label tapes. Among these are semi-gloss lithographic, glossy clay-coated, tag paper, ledger paper, foil paper and weatherproof paper tapes. Available in assorted colors, all are designed for easy stripping.

Circle 50 on Card Facing Page 51

Power Sweeper

Better dust control, wider sweeping path and hydraulic dumping are features of the new power sweeper developed by G. H. Tennant Co. Improved dirt pickup and dust control



result from a specially engineering brush-and-vacuum system. A revolving 42-in. main brush throws dirt, debris and litter directly forward into a 12-cu ft dirt hopper. This floating-type hopper rises when bulky litter is encountered, allowing on-the-run sweep-up of large objects without interruption. A high-volume vacuum fan sucks dust from the sealed-to-floor brush compartment, trapping it in a large 4200-sq in. filter area fabric bag.

Circle 51 on Card Facing Page 51

Position Lock

The F410 position lock, manufactured by The Bassick Co., for castored



trucks, scaffolds, and other mobile equipment incorporates a kick release bar as standard equipment. A downward pressure on the pedal sets the shoe securely in contact with the floor and a downward pressure on the kick bar releases the lock.

Circle 52 on Card Facing Page 51

Tilt Type Cab

Diamond T Motor Car Co. offers its new Tiltcab Model 430C. This vehicle carries a GVW rating of 20,000 lb, and with optional oversize rear axle, is rated at 22,000 lb. Engine accessibility in this new model is unique. For

service or preventive maintenance work, the complete cab, cowl and fender assembly is counterbalanced to tilt forward manually without the need of a power unit of any kind. The entire area of the radiator, engine,



clutch and transmission is completely opened up for ready accessibility to any part. Tilting action takes only a few seconds, and is easily handled by the driver alone.

Circle 53 on Card Facing Page 51

Dust-Resistant Trucks

Dust-resistant construction is available on the complete line of electrically-powered materials-handling trucks produced by Lewis - Shepard Products, Inc. These trucks are protected against the effects of metal, coal, coke and grain dust, as well as carbon black. Both the traction and pump motors are enclosed and provided with special, extra-wide, wrap-around brush covers and cork neoprene gaskets to provide tight sealing.

Circle 54 on Card Facing Page 51

Shipping Stencil

A shipping stencil, coated with carbon on one side to permit typing or handwriting with business forms, has been developed by Weber Marking Systems, Div. of Weber Addressing Machine Co., Inc. Called the Carbon-



Cote Tab-On-Stencil, it is tabbed over the Ship-To area of the order-invoice or bill-of-lading form and prepared as a by-product of the forms writing, and secured to address labels or carbons.

Circle 55 on Card Facing Page 51
(Please Turn Page)

DA NEW PRODUCTS and EQUIPMENT

CONTINUED FROM PREVIOUS PAGE

Typewriter-Accounting

Burroughs Division of Burroughs Corp. is offering a new office machine combining an 84-character, full-key-

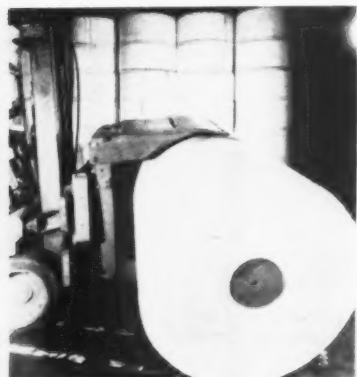


board electric typewriter with a high-speed, accumulating register accounting machine called the Typing Sensimatic. This new machine, available in 10 models, is designed to meet demands for flexibility in the handling of office paperwork in the posting and accounting areas.

Circle 56 on Card Facing Page 51

Paper Roll Grabs

Cascade Mfg. Co.'s Roll Grab has proved itself a sure, non-slip handler of high-finish rolls weighing up to 6500 lb. The powered swinging arm



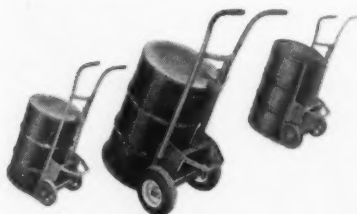
provides automatic adjustment for a wide range of roll diameters. Drivers no longer dismount to position the

arm for different roll sizes. Paper roll grabs are made for every paper roll handling operation, and are available in three models with rated capacities of 2000, 4000 and 6500 lb. In addition to the powered swinging arm, there are three sizes of rigid arms for each model.

Circle 57 on Card Facing Page 51

Barrel Truck

Palmer-Shile Co. is manufacturing a new barrel truck for easy handling of barrels weighing up to 1000 lb. To load, shove truck up against barrel or drum, drop the sliding steel catch



down over the rim, pull truck handles toward you and it will load automatically without rocking or tugging. It weighs only 85 lb and is constructed of heavy tubing and welded steel parts with two roller bearing wheels.

Circle 58 on Card Facing Page 51

Curved Conveyor

Power-driven curve conveyor, manufactured by Power-Curve Conveyor Co., conveys bags, cartons, bundles or packages around turns of any length under power. Standard widths are 18 in., 24 in., and 32 in., with one belt roller for each 10 deg of curvature.

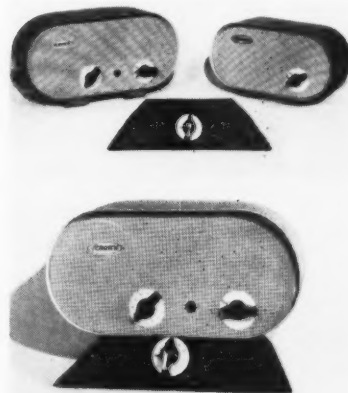


Only 7 in. of headroom is required, with no take-ups or return idlers and no special head or tail pulleys. It runs either way and reverses instantly.

Circle 59 on Card Facing Page 51

Inter-Com

A flexible, easy-to-install line of inter-com equipment has been introduced by Crown Controls Co., Inc. Features of the master unit are tone

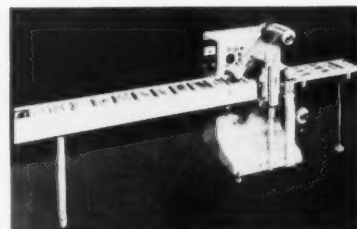


signal for calling the stations, three tube high-gain circuit, low hum level, 4-in. special speaker, on-off pilot light indicator and wide-range volume control.

Circle 60 on Card Facing Page 51

Packaging Machine

Adjustable, flexible packaging machines have been developed by Battle Creek Packaging Machine, Inc. Called Flexopaker because of their adjustability at speeds ranging from 40 to



100 packages per minute, it is unnecessary for users to buy special, custom-made pouch-forming dies for each size package being handled. Package-size can be adjusted on all three models, two of which feed horizontally and one which feeds vertically—from a minimum size of 1½ in. by 1½ in. by 1/16 in. to a maximum of 27 in. by 15 in. by 2 in., with a multiplicity of sizes possible between these.

Circle 61 on Card Facing Page 51

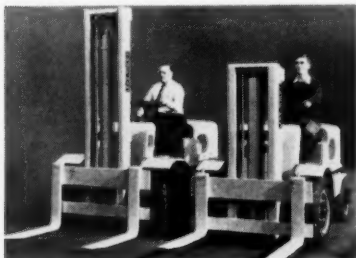
Roof Coating

The problem of leaky roofs is solved by the Flexrock Co. with their new product, Rooflex. It is used for quick permanent leak repairs. It can be applied while it rains, forming a water-tight, seamless seal. It will withstand water, smog and chemical fumes.

Circle 62 on Card Facing Page 51

Multi-Lift Mast

Truck-Man Division, the Knickerbocker Co., announces the Multi-Lift mast available on all its high lift fork truck models. It will provide a much lower truck silhouette while maintaining equal or higher lift. Any Truck-Man fork truck equipped with a



Multi-Lift Mast having a 144 in. lift has a lowered height of only 72 in. This is at least 20 in. lower than the overall height of conventional masts having the same lift. There are no latches, ratchets, telescoping cylinders or other complex mechanisms.

Circle 63 on Card Facing Page 51

Hard Surface Flooring

Flooring material compounded especially for use in high speed hand and power truck locations, is introduced by The Monroe Co., Inc. Known as Sternite, it is applied in a ¼-in. layer over concrete and forms an extremely hard surface that eliminates all wheel drag. Will not depress under wheels. Is dark brown in color and said to be non-slip, non-porous, lightweight, fire-retardant, wear resistant, easily applied and feather-edged.

Circle 64 on Card Facing Page 51

Package Conveyor Belt

Ribbed conveyor belt that will carry lightweight packages as firmly as it carries heavy cartons has been announced by B. F. Goodrich Industrial Products Co. Griptite is designed for steep-angle conveyor operations in stores, factories, warehouses and shipping rooms that handle both light

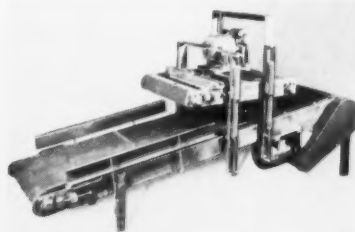


and heavy merchandise packages. Standard ribs are ¼ in. high and are spaced four to the inch. The raised ribs, which move packages onto the belt, are ⅜ in. high.

Circle 65 on Card Facing Page 51

Bag Flattener

Sage Equipment Co., Inc., manufactures a bag flattener which removes the air and compresses a bag so that stacking is easier. Both upper and lower decks are power driven. The



amount of compression is arranged by adjusting the upper deck spring tension control. Standard units are available in 16, 20, and 24-in. widths and 8, 10 and 12-ft lengths.

Circle 66 on Card Facing Page 51

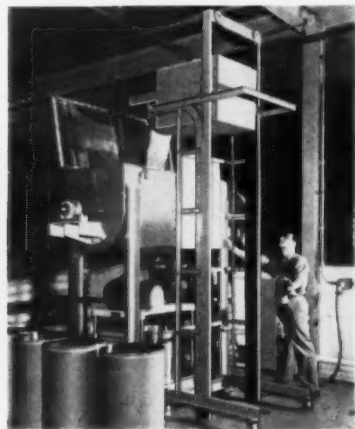
Pocket-Size Radio

A commercial pocket-size frequency modulation (FM) radio receiver for mobile communication service is announced by Radio Corp. of America. This micro-miniature receiver is a fully transistorized, 10-oz instrument designed to provide extensions of several miles for radio systems now operating on the 150-megacycle band. It measures only 2¾ in. wide, 1 in. thick, and 6½ in. high; and operates with self-contained flashlight-type mercury batteries. It operates on a fraction of a watt of power.

Circle 67 on Card Facing Page 51

Lifter-Dumper

A mechanical lifter-dumper, manufactured by Essex Conveyors, Inc., has been announced. The SESCO Jr. is a fast lightweight machine capable of handling 75 400-lb loads per hour. It can be equipped as a multi-purpose

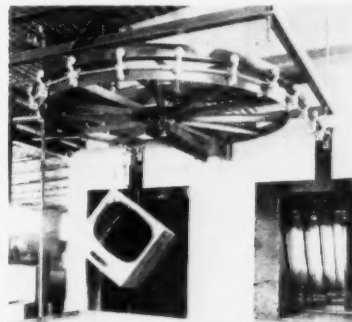


machine with changeable skips for handling drums, barrels, boxes, and bags. These new models are made in various sizes to dump at any height ranging from 2 to 8 ft.

Circle 68 on Card Facing Page 51

Caterpillar Drive

A new caterpillar drive for cable conveyors manufactured by The E. W. Buschman Co., increases the application of overhead cable trolley conveyors. Applied on the straight run of conveyors, this drive allows large-radius traction wheel turns to be used. As a result, sizeable loads can be handled on close hook centers. This caterpillar drive has two strands of precision chain with special dogs that engage the cable and trolley with



equal pressure from both sides. Side thrust is carried by both chains, so that cable movement is transmitted smoothly during engagement.

Circle 69 on Card Facing Page 51

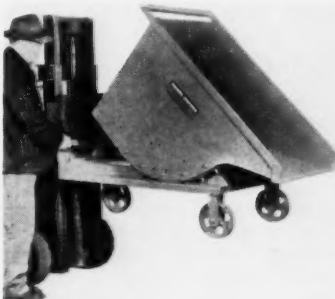
Multiwall Sack

A low-cost multiwall sack featuring aluminum foil laminated to kraft paper has been introduced by Hudson Pulp and Paper Corp. Designed for products requiring maximum protection against moisture, odor, and gases, the sack uses a sheet of .00035 in. foil laminated to 40-lb kraft positioned between the inner and outer plies.

Circle 70 on Card Facing Page 51

Self-Dumping Hopper

A new self-dumping hopper only 30 in. wide, designed for handling bulk materials, scrap, and waste through



narrow aisles and in confined areas, now is available from Roura Iron Works, Inc. The hopper constructed of heavy steel plate will pass through ordinary doors with ease, and has a capacity of 10 cubic ft.

Circle 71 on Card Facing Page 51

FREE

LITERATURE

Transporter Data

Automatic Transportation Co. has prepared a booklet giving complete data on Transporters, its line of walkie lift trucks. Several case histories of specific installations and detailed facts on lease or purchase are included.

Circle 72 on Card Facing Page 51

Adjustable Locks

A bulletin giving details on the adjustable Span-Locks manufactured by Magnesium Co. of America is available. It shows where and how the new Span-Locks can increase your dock loading efficiency and safety.

Circle 73 on Card Facing Page 51

Truck Index

Revolvator Co. is offering a Truck Index of over 600 combinations of sizes and capacities of industrial truck models. Illustrated in this index are basic types of both Red Giant hand lift and Go-Getter electric lift trucks.

Circle 74 on Card Facing Page 51

Conveying System

A booklet describing Lamson Corp.'s pre-engineering Erecto conveyor units now is available. How to save engineering and installation costs with this conveyor unit is explained.

Circle 75 on Card Facing Page 51

Labeling System

Weber Marking Systems, Div. of Weber Addressing Machine Co., is offering a bulletin on its Facsimile-Label System, a fast, systematic way for addressing multiple shipments. You can imprint both a facsimile label and customer's address directly on a carton. From 30 to 40 cartons can be addressed in a minute.

Circle 76 on Card Facing Page 51

Furniture Pads

An illustrated folder describing the complete line of Iron Horse furniture pads, moving equipment and supplies is available from the Canvas Specialty Co.

Circle 77 on Card Facing Page 51

Off-Highway Truck

An eight-page, illustrated folder describing the 24-ton "95" and the 18-ton "65" Payhauler, off-highway trucks, is available from International Harvester Co. Also included are specifications of the two machines.

Circle 78 on Card Facing Page 51

Steel Pallet Boxes

An illustrated 34-page report on how to cut materials handling costs by the use of Cargotainers, welded wire mesh pallets and bulkheads, is offered by Pittsburgh Steel Products, a division of Pittsburgh Steel Co. The unique use of wire mesh fabric as conveyor guard also is discussed.

Circle 79 on Card Facing Page 51

Wheel Conveyor Catalog

Sage Equipment Co. has issued a gravity wheel conveyor catalog, No. SA-102A-1. Many models and sizes, both steel and aluminum, in 5-, 8-, and 10-in. sections are illustrated and described.

Circle 80 on Card Facing Page 51

Pump Selector Chart

A pump selector chart which aids industrial, commercial, and other users of fluid-handling equipment in choosing the model, or models, best suited to their particular needs, is contained in a new catalog data folder on pumps, now available from Barnes Mfg. Co. Among the units covered are gasoline, electric, diesel and belt-driven pumps including sump, diaphragm, standard and self-priming models.

Circle 81 on Card Facing Page 51

Automatic Writing Machine

Commercial Controls Corp. is offering a 24-page booklet, "The Punched Tape Story," describing the new Friden Flexowriter automatic writing machine. The booklet shows how common language punched paper tape can be created by Flexowriter for integrating other business equipment or used to actuate the same or other Flexowriters to produce documents, completely automatically, at 100 words per minute.

Circle 82 on Card Facing Page 51

Pallet Truck

The Raymond Corp. is offering a bulletin on its lightweight, hydraulic pallet truck, Model L2P. It is a completely precision hydraulic unit of aluminum alloy construction. It has a one-piece welded steel frame, safe dual-purpose brake, and 270-deg turning radius.

Circle 83 on Card Facing Page 51

Hydraulic Lift Truck

Big Joe Mfg. Co. announces a new, illustrated 24-page catalog describing the latest Big Joe developments and providing factual information on its complete line of manually propelled, battery operated hydraulic lift trucks.

Circle 84 on Card Facing Page 51

Inflatable Dunnage

United States Rubber Co. is offering information on its new Shor-Kwik inflatable dunnage. It cuts labor and material costs, cushions shipments, speeds loading and unloading, according to the maker.

Circle 85 on Card Facing Page 51

Handling Hints

Elwell-Parker Electric Co. is offering a new quarterly external house organ, the Elpar Lift For Industry. It contains an article on preventive maintenance for industrial trucks and includes work charts which may be used for such programs. Also included are stories describing how specific handling problems have been solved through modern handling techniques, etc.

Circle 86 on Card Facing Page 51

Furniture Van

Fruehauf Trailer Co. has available a data sheet on its furniture Volume-Van giving complete specifications of the new unit, together with information on construction and usage.

Circle 87 on Card Facing Page 51

Portable Lift

The Oster Mfg. Co. has prepared a booklet on its portable lifts. This new lift performs many of the functions of higher priced handling equipment.

Circle 88 on Card Facing Page 51

Rubber Tired Wheels

A bulletin explaining why rubber tired wheels are better suited than any others offered today for floor truck, trailer and caster applications is offered by **Nutting Truck and Caster Co.** Their easy-to-replace tire feature, which cuts operating and maintenance costs, is thoroughly illustrated step by step, according to the literature.

Circle 89 on Card Facing Page 51

Pneumatic Conveyor

A brochure describing the Hoffco-conveyor systems, the **U. S. Hoffman Machinery Corp.**'s systems for pneumatic conveying of dry, free-flowing materials has been prepared. It gives reasons for installing this system, types available, and a run-down on the details of installation and possible application.

Circle 90 on Card Facing Page 51

Belt Conveyor

A bulletin explaining and illustrating the new **Tru-Track** steel belt conveyor is offered by **Sandvik Steel, Inc.** It tells how V-ropes bonded to the underside of steel conveyor belts travel over grooved sheaves to maintain proper belt alignment.

Circle 91 on Card Facing Page 51

Storage Racks

The **Jarke Mfg. Co.** is offering a free set of miniature plastic storage racks which are actual 8-to-1 scale models of their Mini-Module prefabricated stock storage racks. Mini-Module racks are constructed of heavy-duty steel and nest into each other.

Circle 92 on Card Facing Page 51

Packaging Economy

"How to Ship More Economically in Corrugated Boxes" is the title of a 24-page booklet available from **Hinde & Dauch.** It includes pertinent advice on designing, testing and storing of product packages. It offers information on the planning of the shipping department and economy considerations in the packing, sealing, warehousing and shipment of corrugated boxes.

Circle 93 on Card Facing Page 51

Truck Crane

A fully illustrated, four-page bulletin, describing **Tubar** cranes—including capacities, dimensions and full specifications—has been prepared by **Uhrden, Inc.** Tubar cranes feature built-in extension boom, four-point suspension, variable-stroke hydraulic pump, and needle-type control valve.

Circle 94 on Card Facing Page 51

Industrial Truck Battery

Gould-National Batteries, Inc., is offering Bulletin No. GB1567B describing the Gould Thirty industrial truck battery, developed specifically for the sustained power requirements of industrial electrical trucks.

Circle 95 on Card Facing Page 51

Warehouse Containers

Chippewa Paper Products Co., Inc., is offering a bulletin describing their complete line of specially designed corrugated wardrobes and containers.

Circle 96 on Card Facing Page 51

Refrigeration Plates

Truck cold plates that use the vacuum principle to hold desired temperatures are detailed in a new 16 page booklet by **Dole Refrigerating Co.** Instructions for figuring truck plate requirements are included. Graphs and drawings explain the operation.

Circle 97 on Card Facing Page 51

BOOKS

Grocery Handling

"Improving the Truck Delivery Operations of a Wholesale Grocer," a 50-page case study, has been prepared by the United States Department of Agriculture, Agricultural Marketing Service. The study is a complete analysis of the handling and delivering operations of a Baltimore grocery firm—equipment, time and costs are included.

This case study may be obtained from the **Superintendent of Documents, U. S. Government Printing Office, Washington 25, D. C.** Price 35 cents.

Shoreside Facilities Study

Copies of a special study prepared for the Maritime Administration on "Shoreside Facilities for Trailership, Trainship, and Container Ship Services" are available to the public through the Office of Technical Services, U. S. Department of Commerce. The study presents technical analyses, functional requirements and design criteria for the planning of specialized facilities to accommodate roll-on-roll-off, lift-on-lift-off, or conveyor-type terminal operations in connection with new ships of these types.

The study is fully illustrated with technical and pictorial material and may be obtained from the **Office of Technical Services of the U. S. Department of Commerce, Washington 25, D. C.,** at a cost of \$2 per copy.

For prompt service, use postage-free postcard provided to obtain **FREE LITERATURE** and **NEW PRODUCT** information described in this issue. All material is **FREE** unless otherwise noted.

Electric Fork Trucks

A new eight-page brochure describing operational characteristics and mechanical details of its line of electric fork trucks is available from the **Industrial Truck Div. of Clark Equipment Co.** Four major components of electric trucks—control circuits, power train, hydraulic system, and upright assembly—are described and illustrated.

Circle 98 on Card Facing Page 51

Dock Shelters

Dazzo Products Co. is offering literature on its dock shelter which provides a foldable tunnelled passageway between building door and truck or freight car door. It protects merchandise against rain, sleet, snow or driving winds.

Circle 99 on Card Facing Page 51

Bulk Trailer

A four-page bulletin describing a new trailer designed for bulk transportation of sugar and other pulverized and granular materials is available from **Fuller Co.** The folder includes photographs, drawings, and specifications.

Circle 100 on Card Facing Page 51

Palletless Handling

The **Yale & Towne Mfg. Co.** has published a bulletin on its push-pull loader with integral sideshift used for palletless handling. Complete specifications on the attachment and illustrations on the proper procedure for handling without pallets is also given.

Circle 101 on Card Facing Page 51

Plant Safety Kit

Towmotor Corp. is offering a kit of specially prepared safety aids designed to add extra emphasis to any company's plant-wide safety program. In addition to a guide booklet on correct lift truck operation, the kit contains a set of 11 x 14 in. cartoon-illustrated safety posters for display. Also included is a group of colorful directional and warning signs intended to improve traffic flow in congested aisles.

Circle 102 on Card Facing Page 51

Palletized Brewery Eliminates 'Manhandling'

THE O'Keefe Brewing Co., Ltd., of Toronto, Canada, has moved into a new distribution center which, management boasts, is so mechanized that it does everything but deliver and open the bottle for the customer. The completely palletized operation is built around an automatic palletizer and an intricate conveyor network.

The one-story brick and steel building has a 250-ft frontage and is 100 ft deep. The foundation was made heavy enough to support an additional three stories if expansion becomes necessary. Storage capacity is set at 100,000 dozen bottles and 500,000 gallons in bulk.

Tailored System

The building was designed by the company's own engineering staff. The physical structure was built around a handling system tailored to fit the brewery operation.

The main floor is used for full-case storage, shipping, and receiving. Ten covered loading bays provide facilities for loading and unloading trucks. Each bay is equipped with a 10,000-lb electrically operated ramp.

Cases of empty bottles, received by truck, are sorted and sent by conveyor to the bottling lines where they are washed and prepared for re-use.

The basement is occupied by 30 one-piece glass-lined storage tanks with a capacity of 500,000 gallons. Beer is kept before bottling in this cold storage room. The temperature of the room and liquid in the tanks is kept at 32 deg F. Four inches of cork insulation is used in the tank storage area.

The tanks are of two sizes, 11 ft

in diameter and 18 ft long, and 11 ft in diameter by 34½ ft long. Any of the tanks can be linked to the present bottling lines in another building, or to any transfer lines within the plant. Space is provided in this basement for a future carbonating room and bottling cellar.

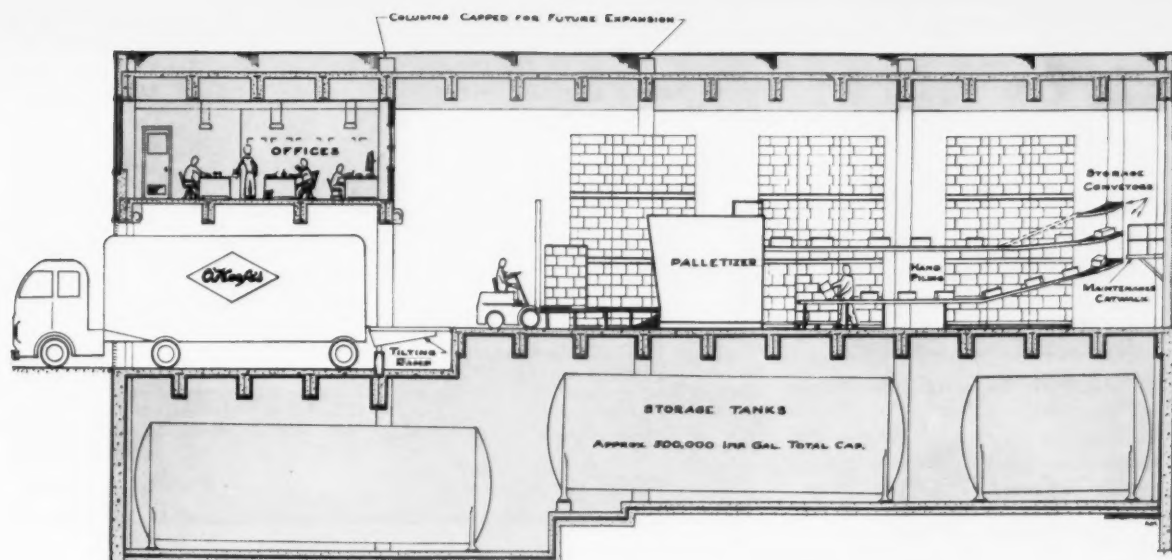
The output of three bottling

Completely palletized handling system in this brewery's new distribution center is built around an automatic palletizer and a gravity and live-roller conveyor system

lines, packed in cases, flows into the building on three separate conveyor lines. A switching arrangement, located on a bridge between the new building and the adjoining buildings, where bottling is carried out, enables the operator to control the output of the conveyors and alter the flow direction as required.

Three conveyor lines feed this automatic palletizer, which discharges pallet loads for fork truck removal to storage or the shipping dock





Cross section shows bulk storage in basement, full goods storage and shipping on first floor, and office facilities on mezzanine

The three conveyors carry the cases to an automatic palletizer. The palletizer counts the cases by means of an electric eye as they flow onto a conveyor belt. It will handle either 12- or 24-bottle cases. The machine automatically changes the position of the cases so that when stacked on a pallet, the full weight of each case is not

directly on the case immediately below it.

The principle is similar to that of laying bricks. On each pallet are placed six layers of cases. Each layer includes 20 12-bottle cases or 10 24-bottle cases. When the pallet is loaded, it is ejected from the machine onto a roller conveyor. Battery operated fork

trucks remove the loaded pallets and stack them three high in the bays. From these bays, they are assembled in a pre-loading area by fork trucks. They then are loaded onto delivery vehicles by low-lift hand trucks.

Package Handling

Cartons travel directly from each bottling unit along separate lines to three conveyors on the bridge between the two buildings. Here, hinged belts give flexibility. Packages coming from any bottling line can be routed either to the automatic pallet loader or to a hand piling station in case of an emergency.

The storage conveyors combine gravity and roller principles. Since a pallet load of 60 two-dozen pint cartons must accumulate before running them to the palletizer, a means had to be established to store this amount of cartons without excessive carton damage.

A straight gravity conveyor would not function well, because it would allow cartons to gain too much speed and travel in trains before stopping with a jolt at the end of the run. Gravity, too, would not provide quick acceleration, but would build up excessive pressure on the accumulated cartons.

(Please Turn to Page 78)

Low-lift pallet trucks are used for loading and unloading over-the-road trucks. Electrically operated loading ramps permit smooth passage from truck to dock





R. M. King



S. A. Kadane



C. J. LaMothe

DISCUSSION groups, panels, seminars, reports, resolutions elections, and social events made the 66th Annual Meeting of the American Warehousemen's Association, held April 1-5 in Atlantic City, one of the organization's most productive.

Following a day of committee and sales group meetings, the first general session opened on Tuesday morning. A panel on labor-management relations was featured. Leading the discussion of "What

Officers Elected at AWA's 66th Annual Meeting

General Officers

President—Raymond M. King, Syracuse, N. Y.
Vice President—Gilbert J. Stecker, Louisville, Ky.
Treasurer—W. A. Morse, Minneapolis, Minn.

Merchandise Div.

Pres.—C. J. LaMothe, St. Louis, Mo.
V.P.—F. D. Bateman, Chicago, Ill.
Treas.—R. C. Greeley, Cleveland, O.
Executive Committeemen—John K. Dozier, Houston, Tex.; Harold A. Drury, Los Angeles, Calif.; and John F. O'Halloran, Boston, Mass.

NARW

Pres.—S. A. Kadane, Dallas, Tex.
V.P.—Philip G. Kuehn, Milwaukee, Wis.
Treas.—John L. Bear, Wichita, Kan.
Executive Committeemen—W. C. Baker, Chicago, Ill.; C. A. Bowen, Jr., Nashville, Tenn.; A. R. Carstensen, Sacramento, Calif.; and G. A. Shoemaker, Elmira, N. Y.

New Labor Outlook

Nation's warehousemen study cost problems,

Will Your Rights As an Employer Be in 1957?", was C. D. Johnston, of Roanoke, Va., past president of A.W.A.

Taking part were Fred A. Hartley, co-author of the Taft-Hartley Labor Management Relations Act; Joseph Moody, president, Southern Coal Producers Association; Philip G. Kuehn, chairman, NARW Committee on Industrial Relations; and John K. Dozier, chairman, Merchandise Division Committee on Labor Relations.

Hartley told the delegates that this law's fault was its not being tough enough rather than being too tough, as its critics claimed. He called on management to stop apologizing for the law and to insist that it be strengthened.

Moody warned that management is going to have to give a lot of attention to government "if we are going to apply the same laws to labor as to companies and others." The laws he referred to are anti-trust regulations.

Kuehn joined in a plea for a stronger Taft-Hartley law. He said that this can only be achieved by pressing and harassing Congress. Another of his suggestions for meeting the labor situation is to continue to sign contracts which do not have hot cargo provisions. Kuehn favors long-term contracts.

Dozier stressed the importance of getting acquainted with one's employees. He pointed out that unions know that they must know their membership. Dozier said that men in industry are not amenable to unions before they are organized.

He also called for an end to the National Labor Relations Board as a method of deciding labor disputes. Most of these, he said, are decided on the basis of expediency rather than facts. He said settlement belonged in the local, elected court.

Reports by President M. W. Young, of San Francisco, and



Raymond M. King (second from left) accepts the gavel from M. W. Young, of San Francisco, Calif., retiring president. W. A. Morse and G. J. Stecker, treasurer and vice president, look on

Studied By AWA

elect officers during four-day annual meeting

Treasurer W. A. Morse, of Minneapolis, completed the morning session.

The two divisions of the organization—the National Association of Refrigerated Warehouses and the Merchandise Division—met separately for the first time during the annual meeting Tuesday afternoon.

NARW President G. J. Stecker, of Louisville, and Merchandise President C. J. LaMothe, of St. Louis, each reported to his respective group.

Other reports in the Merchandise Division were presented by Executive Vice President Donald E. Horton, of Chicago, and Treasurer R. C. Greeley, of Cleveland.

Written committee reports were distributed to Merchandise Division delegates and a brief discussion of each was held. Chairmen of the committees preparing the reports were R. C. Schall, Banking Relations; Banfield Capron, Business Promotion; A. M. Lownsbury, Plant and Operations; C. O. Butler, Uniform Commercial Code; John K. Dozier, Employee Relations; F. D. Bateman, Government Relations; and Bruce S. Howard, Port Terminals.

The Merchandise Division session ended with C. B. Taylor, of Toronto, discussing "A Proposed Cost Accounting Manual Program."

Other NARW reports included predictions of storage for the next year prepared by chapter chairmen. These were A. W. Oakley, Jr., North Atlantic; W. J. Lamping, Great Lakes; R. L. Smith, Missouri Valley; W. J. Mills, first vice president, South Pacific; Alex P. Olsson, chapter representative, North Pacific; R. T. Mackenzie, Southwest; and Virgil Weekley, secretary-treasurer, Southeast.

Maj. Gen. Henry Ray McKenzie, executive director, Military Subsistence Supply Agency, talked on "What's New in Perishable Subsistence" and H. Cochran Fisher,

NARW insurance counsel, spoke on "Profit Planning with Insurance."

In the Wednesday session of the NARW, sales and labor were the fields of interest. Dr. Kenneth McFarland, educational director, American Trucking Associations, Inc., spoke on "The Man in the SalesMAN."

The second part of the program was a panel on "Labor's Plans for Refrigerated Warehousing." Kuehn was the moderator. The panel consisted of J. P. Johnson, chairman, NARW Legislative Committee; H. W. Wilson, Philadelphia; H. M. Sprading, Carthage, Mo.; Dozier; P. V. Henningsen, Portland, Ore.; W. W. Wilson, Jr., Brooklyn; L. J. Urban, Los Angeles; and William Jobe, Jr., NARW special assistant for legislation and industrial relations.

Committee reports were presented by Jack Keefe, Bridgeton, N. J., safety; E. E. Hesse, Chicago, insurance; V. C. Stevens, Dallas, warehouse operations; P. B. Christensen, Portland, Me., another phase of warehouse operations.

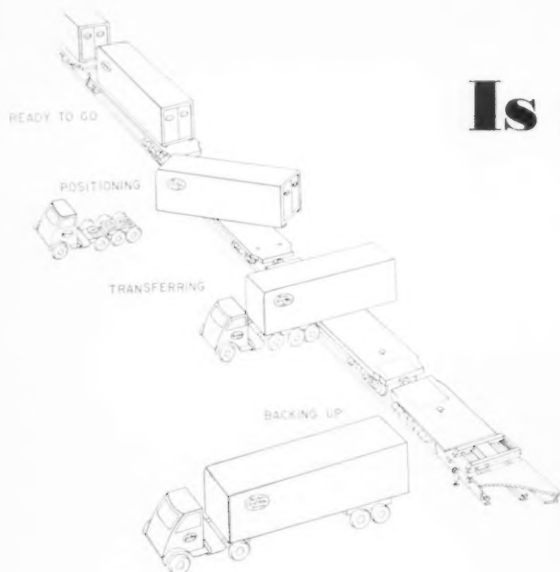
Speakers before the Merchandise Division on Wednesday morning were Morris M. Stern, Kansas City, chairman, Office Procedures Committee; Leigh Davis, Louis Behr Organization, Cleveland; George S. Odiorne, American Management Association, New York, and Robert H. Jacobs, Jacobs Construction Company, Jacksonville, Fla.

"New Developments in Warehouse Documentation and Office Mechanization" was Stern's subject. Davis spoke on "Pension and Profit-Sharing Plans."

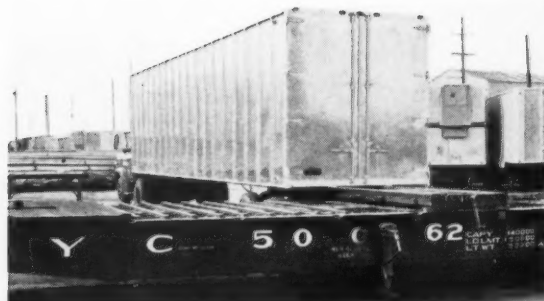
Odiorne discussed the topic "What's All This Talk About Automated Warehouses?" "Building a Warehouse in 1957" was Jacobs' talk.

Informal discussion groups were held in each division Wednesday afternoon. These were Bonus Sessions for persons not taking part

(Please Turn to Page 79)



Is the Boxcar Over



Fully loaded Fruehauf-Strick highway trailer is backed into position at right angles to standard railroad flat car

New piggy-back technique, separating wheel assembly from a loaded highway trailer before placing van on a railroad flat car, is seen as replacement for box car

THE end of the old-fashioned railroad boxcar, which he predicted many months ago, may be coming sooner than expected, according to Roy Fruehauf, president of the Fruehauf Trailer Co.

This was indicated recently when the New York Central System unveiled its new truck-trailer-flat-car "Flexi-Van."

This new piggy-back technique separates the wheel assembly from a loaded highway trailer and places the trailer, without wheels, aboard a railroad flat car. Only slight modifications in trailer, wheel assembly, and flatcar are required in order to make the shift.

Quick Changeover

The entire changeover from highway to rail carrier or back

again ordinarily requires only four minutes.

No special docks or expensive lifting facilities are needed. The transfer is made simply by a highway tractor-trailer unit backing up to the side of a flatcar from any road beside a railroad track. A pin release is operated to allow the trailer to slide off the road wheels onto the flatcar lift.

As the trailer is slid across the lift by the backing truck, its center of gravity is determined automatically. When the trailer is properly balanced, it automatically is locked onto the lift.

The road wheels, now under the forward end of the trailer, are completely detached. The trailer, which sits on a hydraulic lift-table, is lifted slightly off the floor of the flatcar. Then it is swung to a position parallel to the

rails and lowered onto the car floor. Again it automatically locks into position.

One-Man Job

Turning the trailer from "trailer position" to "car position" requires little physical effort. The entire operation can be handled by one man.

The receiving mechanism on the flatcar, because of its hydraulic movement, may be adapted to loading from various ground levels beside the tracks.

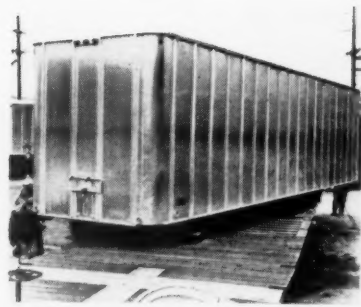
Unlike piggy-back operations requiring special loading facilities, Flexi-Van permits any number of flatcars to be loaded or unloaded simultaneously.

Moved by Man

When the trailer package is completely freed from its highway wheels, it can be pushed by one man into place on the flat car, parallel to the rails. A self-locking device secures the trailer package to the flat car, ready for transportation by rail.

The receiving mechanism on the flat car has a hydraulically-operated vertical movement which may be used to adapt the loading tech-

the Hill?



Trailer, lifted on hydraulic table, is swung to parallel position on car

nique to various ground levels beside the tracks.

Adaption of the Flexi-Van Service to ship loading will be on the same principle. The New York Central Transport Company, a wholly owned subsidiary of the railroad, will handle water-rail operations.

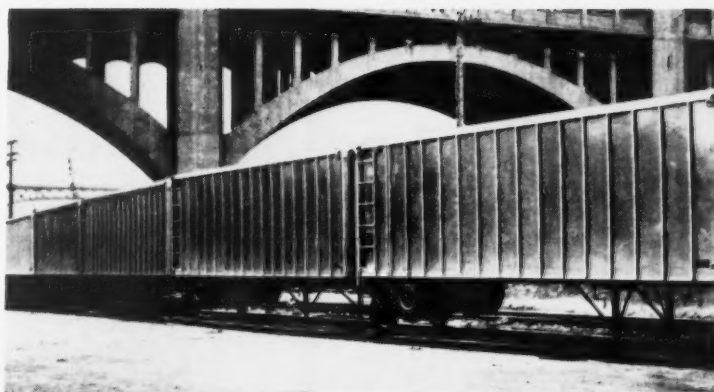
Alfred E. Perlman, president of Central, has announced that after completion of road tests, the company plans to place an order for a large fleet of the new trailers. Rail officials said this order might reach at least 1000.

Light-Weight Cars

Although ordinary flatcars may easily be adapted for this service, the Central plans to have lighter-weight, lower flatcars manufactured for use with the new trailer-package.

Railroad officials expect the new service to begin operating this summer.

These new piggy-back-type operations are seen not only as eventually replacing Central's 35,000 boxcars, but also as possibly replacing present-day mail baggage cars and tank cars in high-speed express service. •



Railvans are capable of conventional freight train speeds and can be joined with standard railroad braking equipment. Ladders are built on both sides

Rail Car Rides Road After 30-Sec. Change

Chesapeake and Ohio Railway introduces car equipped with both rail and highway wheels

A FREIGHT car which can be converted into a highway semi-trailer and back again in one minute is undergoing tests on the Chesapeake and Ohio Railway.

Developed by the C&O, it is equipped with both rail and highway wheels.

Its suspension is designed so that the length of highway and rail arms are easily controlled, assuring the same spring deflection for both highway and rail operation.

An air motor transfers the wheels from highway to rail position.

The transfer operation also serves to level the van for both rail and highway service. One level can be maintained regardless of the weight load.

A rubber spring, developed by B. F. Goodrich Co., is key to suspension

The van is braked by standard railroad air and is capable of conventional freight train speeds. It has lights and reflectors for highway travel as well as ladders on both sides and a running board on the roof to meet all ICC safety appliance regulations. •



Packaging Tied to Traffic at AMA Exposition

Speakers give added impetus to the trend toward coordination of packaging with materials handling, warehousing, transportation — huge savings cited

INDUSTRIAL packaging as an integral phase of the physical distribution cycle was given additional impetus last month at the 26th Annual Packaging Exposition and Conference, in Chicago. The event was sponsored by the American Management Association.

Some 30,000 representatives from more than 9000 companies attended the four-day Exposition. The Conference attracted more than 1000 registrants.

Conference Program

The integration theme was given special emphasis by a panel of five speakers telling the Bendix Aviation Corp. story. Speakers were E. K. Gustin, packaging engineer; R. E. Chrisman, T. E. Shelly, and A. O. Mooney, division traffic managers, and A. S. Love, aircraft service parts manager.

Mooney told the audience that some companies, like Bendix, have coordinated packaging, materials handling, warehousing, receiving, shipping, and transportation with savings of from \$36,000 to \$500,000 a year.

"In many companies," he pointed out, "nearly 50 per cent of the production operation is concerned with the handling of incoming materials and the movement of finished packaged goods to the factory door. This segment needs to be re-examined continuously if a

product is to remain competitive."

Gustin explained this as one of the principal reasons why packaging at Bendix is a function of the Traffic Department.

"The choice of traffic to coordinate packaging activities also reflects growing concern with freight costs," he said. "Packages developed with full knowledge of traffic considerations sometimes pay for themselves in freight savings. For example, palletizing rough steel stampings weighing less than 15 lb instead of shipping them loose has saved more in freight costs than the containers cost. Substitution of other materials for wood in a container for shipping an aircraft component paid for the container by reducing the freight per piece 40 per cent.

"Packaging is one of the elements in classification of materials for freight rates. Therefore the packaging engineer must determine whether the cost of packaging for materials that can be shipped loose offsets the saving in freight rates. He also must consider the materials-handling aspect of receiving loose material.

Rates & Classification

These other reasons for tying packaging closely to traffic were cited by Shelly:

1. Material not packed as provided for in the freight classification can result in a 20 per cent rate penalty.

2. Claims can be declined on the basis of improper packaging.

3. Load factor and susceptibility to damage are considered in setting freight rates.

4. Tare weight is charged for at the same rate as the product.

5. Freight likely to damage other lading because of improper packing may be refused by the carrier.

6. Dangerous articles must be packed in accordance with rules of federal agencies.

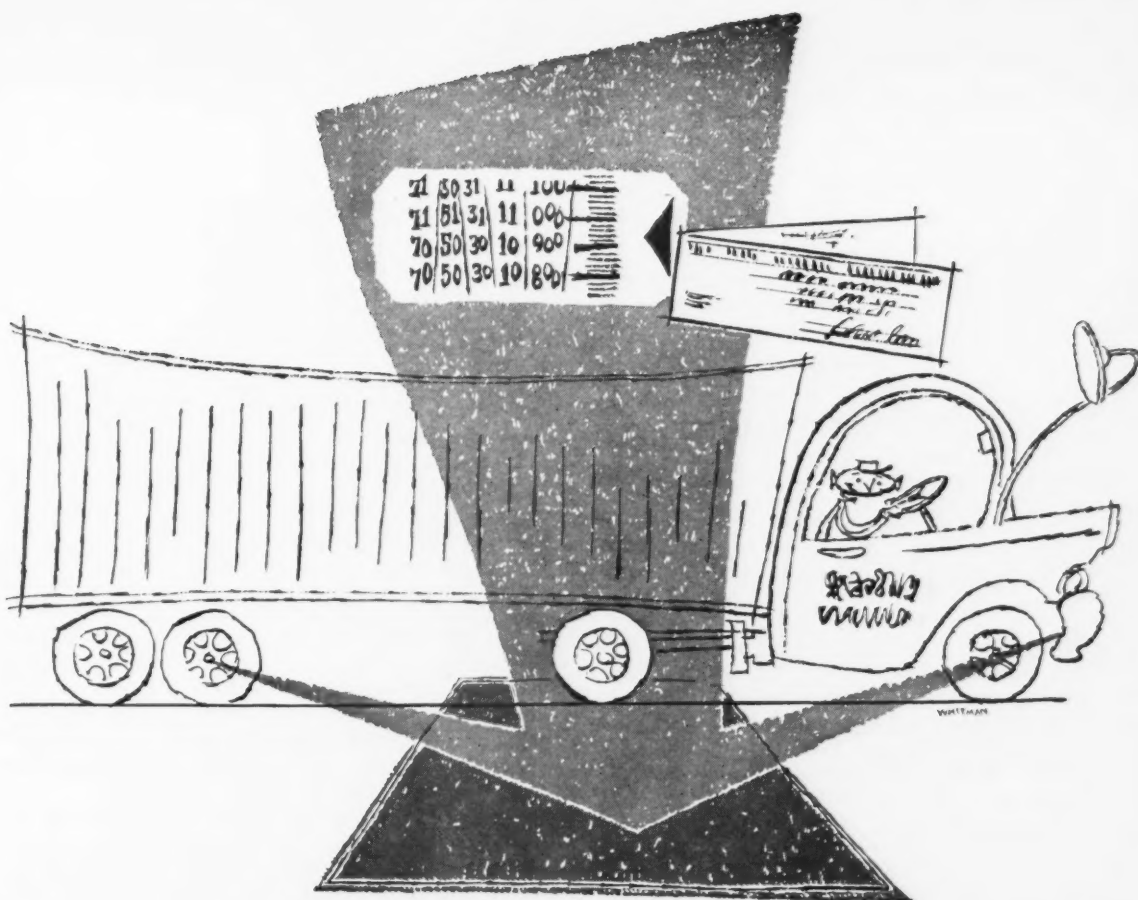
Cost Reduction

Handling of incoming materials, Gustin said, has been improved by increased use of palletization and by cooperative efforts with suppliers to standardize their packaging or tailor it to Bendix's materials-handling facilities.

As a result, he reported, 30 per cent more material is being received with a third less manpower—and with less operator fatigue. Palletizing has reduced the time required to unload a truck from more than 20 man-hours to 20 or 30 minutes, and has reduced boxcar demurrage from about \$100 a month to less than \$150 for the last four years.

His division has found ways to improve on the usually accepted methods and materials prescribed by the armed forces for defense packaging, Chrisman said. For example, aircraft instruments formerly were wrapped in polyethylene-coated kraft paper, bought in roll form. To save the time needed for the operator to estimate the material needed and tear it off the roll, bags of polyethylene in three stock sizes were substituted. This provided added advantage to the customer of a transparent cover

(Please Turn to Page 75)



THE WAY TO WEIGH

▼ IT PAYS to use a truck scale. You get maximum payload. You save on overweight fines. Problem has been a lack of agreement on truck scale installation, use and maintenance. Over 16 months ago, American Trucking Assns.' Regular Common Carrier Conference went to the Scale Manufacturers Assn. and said, "How about it?"

Here's the official answer as worked-out by these two groups, ATA's Operations Council and others involved in truck weighing. Here for the first time is a guide on scale selection, installation, use and maintenance. We think you'll agree that they have done a wonderful job in digesting a mass of data into the following brief concise report—

THE WAY TO WEIGH

Truck users and scale makers worked together, developed this official

SECTION 1

Scale Selection and Installation

Types of Scales

This informative material on the selection and installation of motor truck weighing scales covers two main categories of scales: (a) motor truck scales (for weighing truck and trailer), and (b) axle load scales (for weighing separately the single and tandem axles).

The purposes of the two separate and distinct types of scales are entirely different:

The full length motor truck scale is for determining the total weight of the vehicle, including both tractor and trailer combined, and can, of course, be used to weigh the tractor or the trailer, separately. For a two axle vehicle this scale may be used also to weigh the separate axles.

With the vehicle correctly brought on the scale the long way (for both truck and scale) the middle axles of a three or more axle vehicle cannot be weighed correctly on a full length motor truck scale. The reason is simply that with the vehicle in that position, other wheels would be on the scale, in addition to those to be weighed.

The middle axles may be weighed by bringing the vehicle across the scale from the side and resting the middle axles alone on the platform. However, this creates design, installation and maintenance problems, because the side shock of the side approach is not normal. Unless prepared for by design and installation, the shock force may throw the scale out of alignment which could result in gross inaccuracies. And the wear effect will be greater, affecting the accuracy and increasing maintenance costs.

The axle load scale is especially designed to weigh separate axles or tandems. A typical platform length is 10 feet, but may be slightly more or less, as desired. The purpose of this scale is to determine the load concentration on the individual axles and tandems, to avoid overloading or underloading of vehicles going to the highway. This scale cannot be used to obtain the correct weight of the full vehicle. Such "multiple draft" weighing is not accurate, as has been shown by long experience, and the practice is

actually forbidden in commercial transactions by weights and measures authorities (Section R. 19 Scale Code, Handbook H-44, 2nd Edition, National Bureau of Standards).

Special modifications of these two main types are obtainable, such as: (a) a motor truck scale may be had with two sections of live platform and a center section of rigid platform to facilitate weighing individual axles, as well as the whole rig on the same scale; (b) excellent weighing performance can be had from the installation of a set of three truck scales with separate short length platforms aligned in the same pit, for weighing the whole rig, and of each axle or tandem, at the same time; and (c) the installation of axle load scales may be arranged in pairs, at loading bays, etc., so as to give continuous weight indication of the trailer (both front and rear) while it is being loaded.

The selection of the proper scale for the weighing job, correct installation, and the use of approved and recognized weighing practices, is each a necessary factor in obtaining correct weights. For profitable operations, within the highway restrictions, correct weighing is a requisite. The income of carriers and highway controls are based on weight, and thus the scales and weighing practices of the carriers should be completely reliable and so regarded by all parties at interest. This will provide the needed protection and controls for carrier operations and in addition will create good will with customers and enforcement authorities.



Scale Selection

Maximum Weighing Capacity—Every scale has a rated maximum weighing capacity and in the selection of either full truck or axle load scales it is good economy to install scales with capacities reasonably in excess of anticipated needs, even though the initial cost may be somewhat greater. This will promote longer life of scale parts and will also tend to insure against early obsolescence from inadequacy of capacity.

story on truck scale use and maintenance

Scale parts may be broken or damaged, with resultant inaccurate weighing, when a scale is overloaded. Overloading may occur from excessively loaded vehicles or when vehicles not to be weighed pass over the platform. This may actually disable the scale, or even worse, may just cause it to weigh inaccurately. Such deficiencies in scales, unlike other machines, are not easily observable, and may go on for some time with serious consequences. An oversize capacity scale greatly reduces the chance of damage from overloads or unweighed traffic over the scale, as the heavier parts will stand the wear and strain better than lighter parts of the smaller capacity scales.

Axle load scale capacities generally are of 20 ton or 30 ton. The selection will depend on such circumstances as, the loads to be weighed, the amount of traffic over the scales, etc.

Motor truck (full length) scale capacities are available in four-section types in capacities of 50 ton and over, and in the smaller platform types in capacities up to 30 ton.

Weighing platform size—The scale platform selected should be long enough to accommodate the longest vehicle to be weighed in the case of the full truck scale, and all wheels of the tandems on axle-load scales. The platform area should be ample to assure that all wheels will be resting well within the load platform. Not only will this avoid incorrect weighing, but it saves vehicle and labor time in "spotting" the truck, on the scale.

For axle load scales the platform sizes generally available and found to meet terminal requirements are 8 by 10 ft and 10 by 10 ft. The spacing of tandem axles, of course, is a guiding factor in the length selection.

For full length vehicle scales, the four-section types usually needed at terminals are available in lengths of 45, 50 and 60 ft, all with 10-ft width. In the two-section scales, several lengths are available up to 40 ft, most of them with 10-ft width, but some smaller lengths with 9-ft width. With the long combinations now on the highways the longer platforms of the heavier four-section scales are best adapted for terminal use.

Weighing elements—Two basic types of weighing elements are available, a mechanical lever-type system and a load-cell or electronic system. They can be combined to offer features of both. While most of

the vehicle scales currently in use are of the pit-type lever principle, much development work is in progress on load-cell and electronic truck scales. Various types of such units are in operation and reportedly are performing satisfactorily. Selection of any system should be made to best meet the needs of the terminal.

Platform material—Axle load and full truck scales are designed for the installation of either timber or concrete platforms. Concrete is recommended, as it is more permanent and is more easily kept clean.

Weight indicators—The selection of the indicating element—for reading or recording the weight of the load—is a matter of individual choice, influenced by the number of weighings at the terminal, as well as by the system preferred and the degree of automatic operation needed or desired.

Both full truck and axle load scales can be provided with many types of indicating elements—compound beams, recording beams, optical projection indicators, dials, printers, and remote dials and printers located at some distance from the scale. These latter are connected by electrical cable to a sensing unit (s) at the scale. Also, the indicators and recorders can be connected with punch card, punch tape, and data processing machines.

A small terminal with only a few weighings a day may find an ordinary weigh beam indicator adequate. As the number of weighings per day increases, the convenience of an automatic device becomes apparent. The use of a printed or recorded weight indicator will depend on the system of records used at the particular terminal. Printed or recorded weight is useful as original evidence of all transactions and of the axle and full combination weight in highway matters.



Choice of Location

Inside or outside—As to the choice of locating vehicle scales inside the terminal building or in the yard outside, if it is possible to choose, the inside location has several distinct advantages. Maintenance costs and errors due to snow and ice collecting on the platform and around the edges are greatly reduced with an inside location, as are errors due to wind pressure on the vehicle when weighing.

However, regardless of these recognized advantages for the inside location, vehicle scales are ruggedly built instruments, and with proper installation and maintenance, can provide splendid performance at outside locations.

If possible to do so a location side should be selected which will avoid traffic of unweighed vehicles across the scale platform. Unweighed vehicles

exert as great a strain and as much wear on the scale parts as those weighed. In fact, the impact from the greater speed with which the unweighed truck strikes the platform may cause considerable damage and weighing errors. Maintenance costs will be greater with excessive traffic wear. Weighing performance will be less satisfactory and the traffic will take its toll on the life of the scale.

Soil bearing and drainage conditions—Some terminals are built on filled ground and others where unfavorable drainage conditions exist, and these points should be taken into consideration in selecting the scale site.

In selecting the location due consideration should be given to the soil bearing conditions, as inadequate arrangements to accommodate the loadings specified by the scale manufacturer may result in expensive foundation cost or later unsatisfactory weighing performance.

The site selected should be such, if possible, that surface water will drain away from the scale pit. Standing water in the pit should be avoided and



surface water running into the pit will make drainage difficult and expensive. Pit water causes deterioration of scale parts, inaccurate weighing and excessive maintenance cost.

Approaches—In addition to the length of the scale, provision should be made at each end for approaches equal to the axle base length of the longest outfit to be weighed. For best results the approaches should be straight and level with the scale platform. This should be arranged whether the scale is located inside or outside the terminal building. Approaches level with the platform are absolutely necessary for axle load scales.

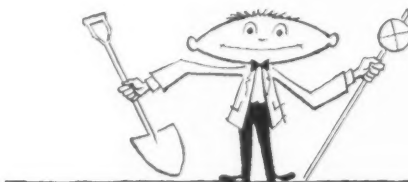
The approaches should be of concrete, black top, or some similar material, to insure that they are maintained in a smooth, level state.

For scales located adjoining the dock, only one approach can be provided, of course, but that approach should be straight and level, and for the length as above described.

Installation width—In addition to the width of the scale platform, normally 10 ft, an additional space of 12 to 18 in. will be needed on each side of the platform to accommodate the concrete pit walls. The exact side requirements will depend on the wall thickness decided upon.

For an indicating element, if it is to be located at the side of the platform, an additional 4 to 5 ft from

the edge of the platform is usually required on either full truck or axle load scales. These additional width space requirements, of course, must be added to the 10-ft platform width.



Scale Foundation

The scale manufacturer will supply a plan for the foundation of the scale, setting out the minimum requirements. The pit should be deep enough and access thereto should be such as to facilitate inspection and maintenance.

Before locating the scale, a check should be made of soil conditions. Precautions must be taken to design and install piers and foundations to support the maximum moving loads to be encountered, considering the load capacity of the soil.

Materials—All materials entering into the foundation and pit walls, such as cement, sand, and rock should be of the best quality obtainable.

Pit construction—It is very important that the piers be correct as to height and their surfaces be smooth and level for supporting the weighing elements of the scale, without intervening supports. The preparation of the pit and the foundation are significant to the accurate performance of the scale and thus should be done by a qualified contractor accustomed to working to close tolerances.

Drainage of the scale pit is highly important, for the protection of scale parts, including those which are critical to accurate functioning. The bottom of the pit should be sloped and connected with a drain leading to a sewer or to a sump from which the water can be removed by a sump pump, preferably of an automatic type.

After the foundation is installed, sufficient time should be allowed for it to harden thoroughly. After such time the forms may be removed, using care not to break the corners of walls and piers.



Installation Details

Supervision—The entire installation of the scale should be made under the supervision of a com-

petent scale erector, preferably a representative of the scale manufacturer, or an experienced scale mechanic recommended by the manufacturer. Even the most costly scale cannot give satisfactory service if improperly installed.

Erection—The foundation should be checked carefully before the installation is started. Any variations from the foundation plans supplied by the scale manufacturer, including the location of the anchor bolts, must be corrected.

Lines should be drawn across the foundation, corresponding to the center lines shown on the plan, and all measurements taken from these center lines. Lever stands must be set as accurately as possible. The levers must be checked for level position with the leveling lugs provided for that purpose. All vertical connections must be plumb. Clearances for working parts must be maintained in accordance with the plan.

Indicating elements—The indicating or weight reading elements of the scale must be securely fixed on a firm and level supporting floor. The usual practice has been to install the indicating element at the side of the scale, but arrangements can be had and plans and equipment obtained for installations at spots remote from the weighing platform.

SECTION 2

Scale Use and Maintenance

Correct Weighing

In correct weighing, the proper use and maintenance of truck scales and axle load scales at terminals are of equal importance with selection and installation of the scales. Improper use of scales, or lack of good maintenance, will certainly result in unsatisfactory weighing results, possibly with serious and costly consequences.

The income of the carriers is actually based on the weight of the burden carried—the charge for carrying a specific number of pounds a certain number of miles. Correct weighing is the key control in the protection of profitable operating income and in addition, in avoiding violations of highway weight restrictions.

Correct Weight determinations can be made only by the use of a weighing scale. For departing trucks to carry the maximum payload and yet not exceed the highway restrictions the loaded vehicle must be weighed, both the full vehicle and the individual axles or tandems.

In addition, the knowledge that a terminal has and maintains dependable correct scales will pay important dividends with customers and prospective customers. Almost everything is bought, sold, carted

and controlled by weight and there is a wide appreciation of accurate weights and scales, throughout the business world. The terminal that keeps reputable scales will be highly regarded, and should be proud that it is scale conscious.



Scale Design

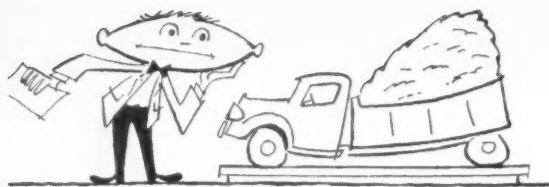
This informative material on motor truck weighing scales covers two main categories of scales; (a) motor truck scales (for weighing truck and trailer); and (b) axle load scales (for weighing the single and tandem axles.)

The purposes of the two separate and distinct types of scales are entirely different:

The full length motor truck scale is for determining the total weight of the vehicle, including both tractor and trailer combined, and can, of course, be used to weigh the tractor or trailer separately. For a two-axle vehicle this scale may be used also to weigh separate axles. To weigh the center axles of a tractor-trailer combination, the vehicle may approach the scale from the platform's long side, if there is approach space. However, care should be exercised to ease the wheels onto the platform to avoid side shocks to the weighing elements. Such use may generally require more frequent inspection to assure correct weighing. Design, installation and maintenance cost is usually greater.

The axle-load scale is especially designed to weigh separate axles or tandems. A typical platform length is 10 ft, but may be slightly more or less, as desired. The purpose of this scale is to determine the load concentration on the individual axles and tandems, to avoid overloading or underloading of vehicles going to the highway. This scale cannot be used to obtain correct weight of the full vehicle. Such "multiple draft" weighing is not accurate, as shown by long experience, and the practice is actually forbidden in commercial transactions by weights and measures authorities (Section R.19, Scale Code, National Bureau of Standards, Handbook H-44, 2nd Edition.)

Both axle-load scales and full length motor truck scales are available with either of two basic types of weighing elements—the mechanical lever system or the load-cell and/or electronic system. Most of the vehicle scales currently in use are of the pit-type lever principle, but much development work is in progress on load-cell and electronic truck scales. Both possess special advantages for particular situations. Selection of either system should be made to best meet the needs of the terminal.

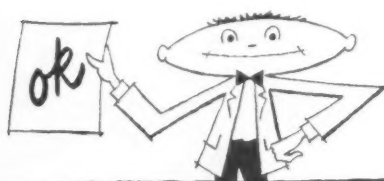


Load Limits

It is generally recognized that the tolerances allowed by the highway laws, above the legal load limits, are permitted in recognition of certain variables in weighing, including load shifts; variation in wind pressure against the truck sides; out-of-level approaches; friction and binds in truck and trailer parts and connections; torsion changes; the effect of braking, and many other factors which affect the gravitational forces being weighed and consequently affect the weight determination of the scale.

In other words, the tolerance allowed by the state is a weighing tolerance and not a loading tolerance for use in loading above the legal load limits. It is inviting trouble and enforcement prosecution to use the tolerance to load above the legal limits. To guess or estimate the weight of a vehicle or the axle loads, also invites trouble. Scales are highly developed and accurate instruments. There is no way to determine weight except by actually weighing on a scale.

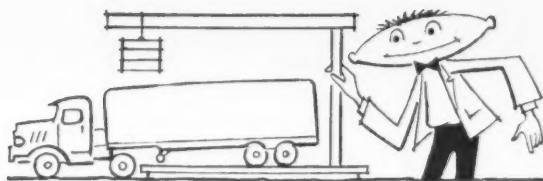
If all terminals and shipping points were equipped with proper and well maintained scales there would be few overload fines and the consequent ill will they create. The small cost per weighing would soon be returned with dividends by the larger payloads and good will which would result.



Weight Tickets

Certified weight tickets in many circumstances of highway enforcement difficulties might serve as evidence which would tend to ameliorate the bad situation. If the driver had in his possession certified weight tickets it would indicate lack of bad intentions. It might serve to establish with the officers that the changes from the original recorded weights were beyond the control of the trucker (provided, of course, there has been no changes of tractor, or load, from that of the combination weighed and recorded on the ticket).

The reputation of the trucker for using good scales and weight tickets will work to his advantage and save time at enforcement stations, etc., and soon repay for all weighing costs. The very fact of such cooperation may create much valuable good will, aside from the protection the weight tickets provide.



Scale Use

Basically a scale is an instrument for the measurement of the gravity force (weight) which is brought to bear on the weighing element or platform. Thus the object to be weighed should be alone on the platform and should be free of any outside circumstance which would influence the gravity force of the object.

For example, in axle weighing, if the approach is not level and in the same plane with the scale platform the force brought to bear on the platform will be above or below the axle's actual weight, depending on whether the other wheels are resting on an approach which is higher or lower than the scale platform.

Responsible Weighman — Proper weighing and supervision of the scale's use and maintenance requires knowledge of scale operation and operating experience. Thus it is advisable to instruct a weighman and, particularly where a large volume of weighing is done, have him alone responsible for all weighing as well as for the daily inspection of the scale. Unless the designated weighman has had previous experience of a satisfactory nature, he should be instructed in proper weighing procedures, and as to the care of the scale. Weighing and care instructions are supplied by the scale manufacturer. Demonstrable advice can be had from the selling or service agency.

Dependable Equipment — Weight determinations should not be taken lightly. The significance and importance of correct weighing to the trucking industry is shown by the use of weight as the basis of hauling charges. For satisfactory weighing results avoid the false economy of inadequate scales and inexpensive installation. A good scale, well installed and properly maintained, will more than justify its cost by its long useful life and accurate performance.

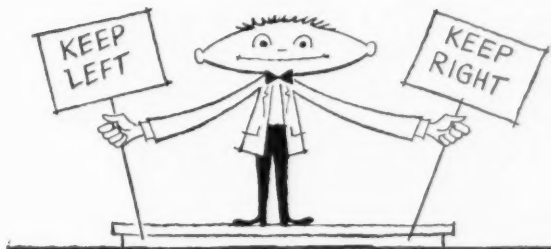
Tests and Examination — Inasmuch as scale deficiencies do not speak for themselves every scale owner is entitled to reports on the examination and testing of the scale at frequent intervals. Like any other machine or device, a scale is subject to deterioration, but unfortunately will continue to perform with no indication that it is out of order. Examinations and tests should be by competent scale personnel, and should be arranged for on the recommendation of the scale manufacturer. Copies (preferably a duplicate of the original) of the test and examination reports should go to the operations manager. Performance tests and operating inspections may be made by state and local weights and measures officials. Such official tests are required in most areas for scales to be used in commercial transactions—

that is, for buying and selling and charging for services.

The testing should be with standard test weights. The test weight load utilized should be as large as practicable, up to the capacity of the scale if possible. One or more "strain" loads may be arranged in combination with available test weights when test weights up to the scale capacity are not available.

Overloading and Unweighed Traffic over the scale should be avoided to promote longer life of scale parts and to avoid actual damage, as well as inaccurate weighing. Every scale has a rated maximum capacity and while there is some margin of safety above that to prevent breakdowns, nevertheless it is recommended that above capacity loads be kept off as they will hasten wear.

Every load which passes over a scale exerts a strain. The strain and wear is greater if the load is not weighed than when it is due to the greater speed and less care. The maintenance cost will be greater, and the accuracy will not be so reliable, on a scale which is subjected to unweighed vehicle traffic. Probably worse than an actual breakdown is the deterioration of a scale in such a situation, caus-

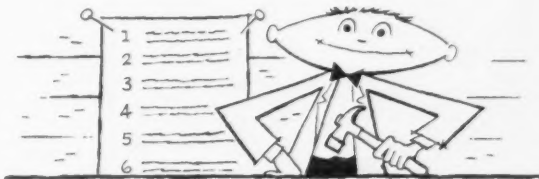


ing incorrect and unreliable weighing results. Weighing deficiencies, unlike those of other machines, are usually not observable. An unreliable scale may go on and on grinding out inaccurate and often times very serious weighing errors which multiplied by the number of weighings over a period of time can be very costly.

"Multiple Draft" Weighing—The method of weighing individual axles or tandems, adding these weights and accepting the sum as the total weight of the vehicle has long been discredited. This expedient, known as "multiple draft" weighing, was formerly used when long platform truck scales were not available. It has been shown by research and investigation of scale people, users, and weights and measures authorities to be unreliable.

The weight of each axle is influenced by numerous factors, and the scale can only measure the force brought to bear on its weighing element. Results of "multiple draft" weighing have been described as similar to what might be expected from a person weighing himself on a bathroom scale with one foot on the scale and one foot on the floor and adding the results of such weighing of each foot. The shifting

and moving of the vehicle changes the forces to be measured. As evidence that "multiple draft" weighing has been discredited, the recommendations of the National Conference on Weights and Measures (the body of officials who regulate commercial weighing devices) condemn the practice (Section R.19, Scale Code, National Bureau of Standards Handbook H-44).

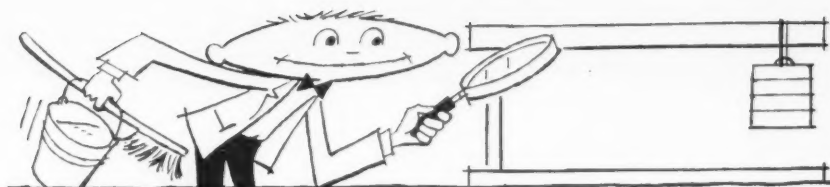


Weighing Procedures

In addition to having a correct scale, properly installed and well maintained, correct weighing requires acceptable standards of weighing procedure, including the following:

1. The zero load balance of the scale should be checked frequently with no load on the platform.
2. All wheels of the vehicle, axle, or tandem to be weighed should be well within the scale platform.
3. The driver of the vehicle should be on or off the platform, as may be required, and this fact should be noted on the weight record.
4. All indicating and recording elements should be handled with care to avoid damage to parts of these precision instruments.
5. Avoid loading the scale above its rated capacity. Also, avoid the passage of excess traffic of unweighed vehicles across the platform.
6. Do not allow trucks to be driven onto the scale at more than a low speed, or the brakes to be slammed on. Such mistreatment may upset adjustments of pivots and bearings, or other scale parts.
7. For determining axle or tandem weights, no wheels should be on the platform except those of the axles being weighed. The other wheels should be straightaway, and resting on the straight scale approach, level with the platform.
8. Relatively small loads, such as 1000 lb or less, are not satisfactorily weighed on a vehicle or axle load scale. Such scales were not intended or well suited for precision weighing of relatively light loads.
9. A suitable record of the weight of the load should be made at the time the weighing is done.

Scale Maintenance



To prevent deterioration of scale parts, to keep the scale in good mechanical condition and to secure accurate weighing results, adequate maintenance practices should be followed, including:

1. It is strongly recommended that where possible the scale owning terminal enter into a contract with the scale manufacturer or a scale service shop recommended by the manufacturer for regular tests and maintenance at intervals not to exceed three months.

The scale mechanic will make actual tests with calibrated weights to find if the scale is performing correctly. He will go into the scale pit and thoroughly inspect the weighing elements. Without such regular inspections by a competent scale mechanic, unsatisfactory conditions may arise to cause serious scale errors, such as: accumulated dirt under working parts to cause friction; rats and birds may nest around or on vital working parts, affecting balance conditions, proper multiplication, etc.

The scale mechanic is trained to know where to look and what trouble to look for. The terminal which entrusts the maintenance of its scales to reliable and competent scale service agencies will be well repaid for the small additional cost of having this necessary service performed by experts.

2. Weight indicating elements should be kept in clean and legible conditions. In climates where heat units are used to avoid corrosive conditions in dial heads, dial inspection is advisable occasionally to observe conditions and see that the heat unit is functioning properly. Parts of indicating elements should always be handled with care as they may be easily damaged.
3. Weight printers, either nearby or remotely located, need care and attention, as do remote indicating and remote control devices. Such maintenance needs the skill of qualified personnel, as does other scale work.
4. Clean the scale platform at least once each day, or as often as is necessary, particularly if snow falls. Examine the opening around the edge of the platform and remove any material wedged in the opening, especially in the winter. Balance the scale at zero after each cleaning.
5. Scale approaches should be maintained in hard, smooth and level condition, and in the same plane with the scale platform.

Distribution Age Editors acknowledge with grateful thanks the generous cooperation of the Editors of Commercial Car Journal, a sister publication, in preparation of this material.

6. The pit should be inspected at regular intervals. Dirt and refuse should not be permitted to accumulate. Foundation floor should be swept clean.
7. Pit water should be quickly eliminated. If the drainage system or pump are not operating efficiently they should be cleared or repaired promptly, as the case may be. Moisture can have rapid injurious effects on a scale.
8. Inspect bumper bolts and check rods occasionally. These should never be tight, but should have $\frac{1}{8}$ in. play in them.
9. Paint scale levers and structural steel members once a year. If rusting should start, thoroughly clean the metal before repainting. Do not get paint on knife edge pivots or bearings.
10. Pivots and bearings should be kept coated with grease of a consistency that will neither melt nor freeze. Old, hard and dirty grease should be removed and the parts be repacked with fresh grease. Never oil pivots and bearings as the oil collects dirt and dust, to cause friction and scale errors.
11. Scale modifications to increase length, etc., of an installed scale are usually expensive and unsatisfactory. Such should be undertaken only with the advice of the scale manufacturer, or a reliable scale serviceman.
12. The grinding of pivots and the repivoting of levers is work requiring specialized tools and equipment found only in well equipped repair shops. Such reconditioning should not be undertaken at the scale location. The parts should be removed and transported to the tooled shop.
13. In the case of load-cell scales, whether electrical, hydraulic, or pneumatic, all connections between load-cells and the instrumentation should be tight and water-proof. In the case of electrical load-cells, it is important that there be no poor contacts on the terminal boards and that the leakage resistances between any of these cell leads, or between any leads and grounds shall not exceed the limits specified by the manufacturer.
14. The best preventive maintenance for the indicator units used with load-cell scales is absolute cleanliness. Dust and dirt in the gears increases the friction in the system and decreases the resolution of the system. Doors and access ports to indicator units should be kept tightly closed at all times. If a blower is incorporated, the filter unit should be cleaned at least once a week.

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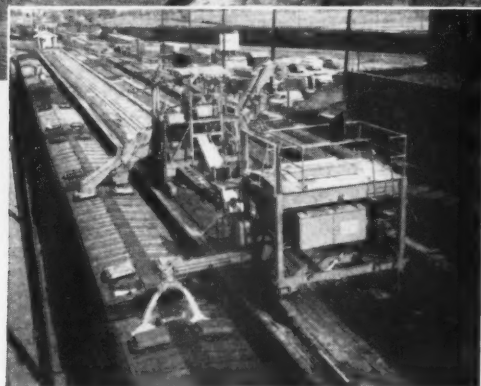
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This mechanical icer is adaptable to most existing docks—services cars of all heights with hatch covers of all types. For full particulars, call your nearby Link-Belt office. Or write for Folder 2643.

14-270

LINK-BELT

ICING EQUIPMENT

LINK-BELT COMPANY: Executive Offices, Prudential Plaza, Chicago 1. To Serve Industry There Are Link-Belt Plants and Sales Offices in All Principal Cities. Export Office, New York 7; Canada, Scarboro (Toronto 13); Australia, Marrickville, N.S.W.; South Africa, Springs. Representatives Throughout the World. Circle No. 13 on Card, Facing Page 51, for more information

Here's a busy fleet that stays in port

Fleet of 14 Allis-Chalmers fork lift trucks speeds handling of shipments at Port of Chicago

There is a steady flow of goods through the Port of Chicago . . . a smooth, two-way interchange between ship, rail and trailer. Expediting this flow is a fleet of 14 Allis-Chalmers fork trucks operated jointly by Illiana Storage Company, Great Lakes Warehouse Corp., Packers Terminal Warehouse Corp., and Seaway Stevedoring Company.

Their choice of trucks was made with an eye to the future as well as the present, for they know this fleet would be called on to work under more demanding conditions when the St. Lawrence Seaway is completed. Already, the wisdom of their selection has been proved by the Allis-Chalmers fork trucks, as is shown by the accompanying letter from Al Williams, Warehouse Superintendent.

Ask your Allis-Chalmers material handling dealer to show you how these fork trucks can streamline your operations, make your handling economical and fast.



Illiana Storage Company, Inc.
GENERAL MERCHANDISE STORAGE TRAN.

January 24, 1957

Allis-Chalmers Manufacturing Company
Milwaukee, Wisconsin

Attention: Mr. C. A. Leets, District Manager

Dear Sir:

The nature of our business is such that very little allowance can be tolerated for down-time due to mechanical failures or poor service.

Uninterrupted handling of material under peak load conditions is essential to our customers and ourselves in this highly competitive business.

Allis-Chalmers fork trucks have made it possible for us to operate at costs much lower than we had thought possible. A thorough understanding by your dealer of warehousing and stevedoring problems have simplified our maintenance problems.

ILLIANA STORAGE CO., INC.

Al Williams
Al Williams, Wsae. Supt.



Negotiating ramps under full load is the all-day job of this Allis-Chalmers fork truck. This is where heavy-duty construction pays off. The industrial-type Allis-Chalmers engine handles this job with a comfortable reserve of power, keeps on working month after month.



Ease of handling accounts for extra work done, especially where precise maneuvering is required. Durability is another reason for the popularity of Allis-Chalmers fork trucks here.

ALLIS-CHALMERS, MATERIAL HANDLING DEPARTMENT,
BUDA DIVISION, MILWAUKEE 1, WISCONSIN

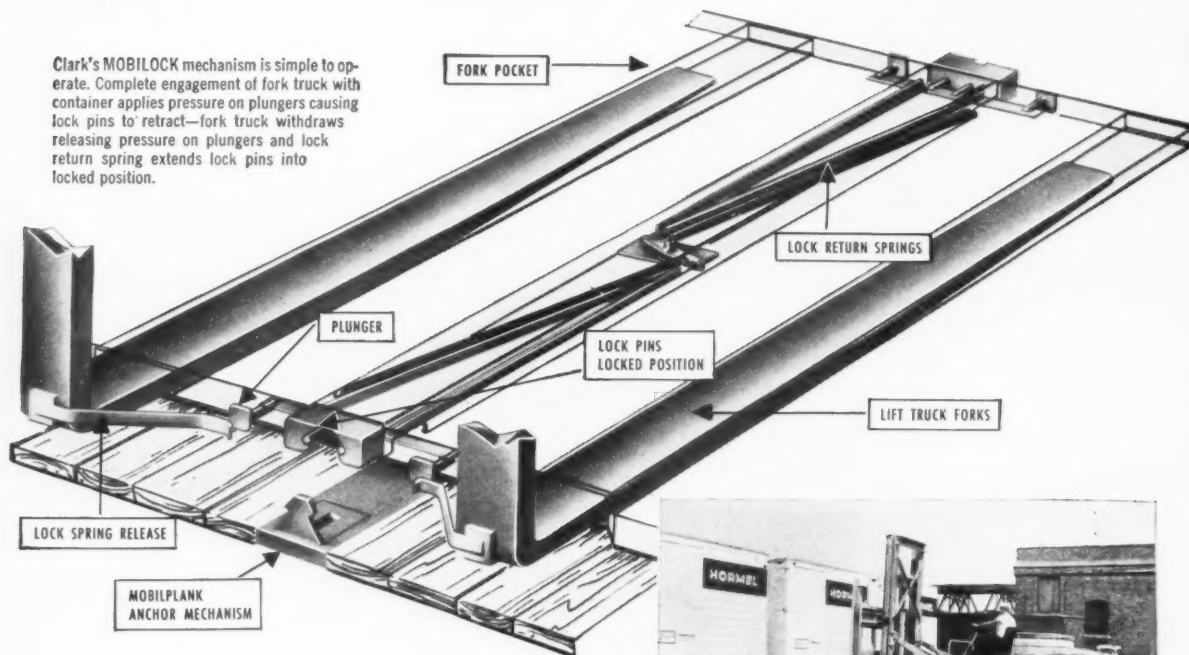
ALLIS-CHALMERS



BH-49

Circle No. 14 on Card, Facing Page 51, for more information

CLARK Mobilock® and Mobilplank®-locking devices-anchor 35,000 lb. load to any carrier... AUTOMATICALLY!



The job of loading or unloading trucks or rail cars is NOW a job measured in seconds. Giant-sized, 35,000 lb. loads are handled in a van type container . . . and the entire job is done by one man.

It's as easy as this:

A loaded container is handled by a Clark fork truck and is placed on rail flat car or truck bed equipped with a Clark MOBILPLANK anchor device. Insert forks—container is automatically unlocked; remove forks—container is automatically locked. At destination the load is removed as easily.

The MOBILOCK and MOBILPLANK devices have been tested and proved with over 35,000 lb. loads and up to 40 g impacts! They are simple in construction, built for rugged duty . . . need *no maintenance*. They self-align and properly seat the container; are adaptable for new or existing equipment; and are interchangeable between carriers. They are low in cost. It is the *only* completely automatic locking system for large containers that lends itself to *standardization* for all mediums of transportation.

For complete details write or wire.

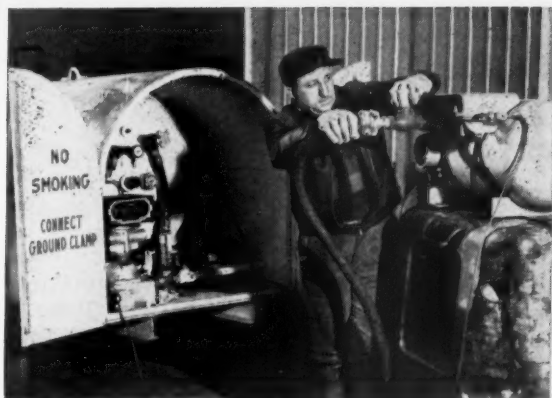


Industrial Truck Division
MOBILVAN System
Trademark of
CLARK EQUIPMENT COMPANY
Battle Creek 11, Michigan

Circle No. 15 on Card, Facing Page 51, for more information



Spector Freight System, Rock Island R.R., Hormel—These users have proved the versatility, the value of Clark's MOBILOCK and MOBILPLANK locking devices for freight handling. Ask about their experiences.



Specially trained operators are in attendance at fuel stations to insure safety. Here fuel trailer is used

Switch to LPG . . .

(Continued from Page 41)

from the experimental period, and on advice of a petroleum company representative, this rule was not observed at the Waukegan plant.

It was found that propane, although it would not cure existing engine ills, would not exaggerate the deficiencies or hasten engine failure. For this reason, the first large conversion program was carried out on all units, regardless of engine condition.

This program was followed even though it would not allow the firm to evaluate fully the total effect of conversion to propane. After conversion, even engines in poor repair compared favorably with their pre-conversion performance.

It was recommended by builders of propane engines to use high-compression heads and larger diameter valves to obtain the original rated horsepower. In the control tests and the initial conversions this was found to be unnecessary. The units were, in general, governed mechanically at lower than the rpm for maximum horsepower. They were governed because of safety rules regulating the operating speed in the plant. The units, therefore, did not warrant the additional expenditure of high compression heads and different valves. No noticeable decrease in effectiveness was noted with units thus converted.

It has been almost three years since Johns-Manville started conversion. Certain gains have been evaluated accurately. In other respects the company knows that gains have been made, but the full importance has yet to be determined. In the first category is fuel cost. The converted units give almost the same number of hours per gallon on propane as they did on gasoline. The cost of propane is considerably less per gallon when purchased in bulk quantities. Experience to date indicates that maintenance costs will be cut at least one-third through the use of propane.

One of the company's 6000-lb fork lifts was made the subject of a special study of engine wear. The engine was torn down for complete examination after 1300 hours of operation on propane. This was the approximate mileage at which a complete overhaul would have been necessary had the engine been operating on gasoline. The wear on the pistons and cylinders was so slight that reboring was unnecessary. The original pistons were put back in. This is the equivalent of nearly 40,000 road miles—and the operating conditions of fork lifts do not give as good engine mileage as we are used to in highway vehicles.

Shop records also indicate an important gain that was not at first expected. The rate of wear on all power transmission units down to and including the tires appears to be less than it was when the engines run on gasoline. This is attributed to the noticeably smoother operation of the engines in the low speed and accelerating ranges.

Life of lubricating oil also has been greatly extended. Operating on gasoline it was necessary to change oil twice each month. Since the fleet has been on propane the drainage period has been extended steadily. It now is considered practical to operate on a twice-a-year drainage schedule.

Break-In Period

Several new units, plus some replacement units, have been added to the fleet, since the conversion to propane. These units have all been purchased with standard gasoline engines and converted in the company's shop. Vaporizers and tanks from the retired units can be used on the new conversion with a saving in capital outlay.

Another reason for buying gasoline equipped units is that the wear-in or break-in period is reduced by starting on gasoline and converting later to propane. The normal procedure is to run the units on gasoline during the warranty period given by the manufacturer, then convert. This system simplifies maintenance by making it unnecessary to stock parts for several makes of carburetors and to train personnel in the maintenance of several makes of equipment.

The first installations at the Waukegan Plant were made with removeable fuel tanks. After it became clear that the entire fleet would be converted, it seemed better to use permanently mounted vehicle tanks and build a service station to which the trucks could be driven for refilling. The filling station is centrally located, and a trained attendant is on duty whenever the plant is operating.

Conversion of the plant fork lift fleet at the Manville location started about 18 months ago. It included the entire industrial

(Please Turn Page)

How to diagnose and cure...

POOR CIRCULATION

in extremities



SurfAir comes in handy
one-bill, one-call,
one-responsibility doses.

SYMPTOMS: Numbness in off-line markets while on-line markets show healthy tone.

DIAGNOSIS: Failure to deliver fresh supplies to off-line outlets at moment of need; poor coordination or bad control of remote carriers.

CURE: New Slick SurfAir service...coordinated surface-air transport through a network of SurfAir participating carriers who deliver lifeblood supplies to off-line points directly from airport...or pick up from off-line points for fast Slick air cargo service. Tone up...write for SurfAir information today!



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and Alarm Service
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and Waterflow Alarm Service
Watchman's Supervisory and
Manual Fire Alarm Service
Industrial Process and Heating
System Supervisory Service
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Holdup Alarm Services

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Central Stations in All Principal Cities

ADT

Circle No. 16 on Card, Facing Page 51, for more information

Switch to LPG...

(Continued from Preceding Page)

truck fleet. Installations were made at the company's general erection and repair garage under the supervision of the foreman. No changes were made in the engines except to take off the gas-line equipment and install the propane fuel systems. No unusual operating problems have been experienced, and the power appears to be almost equal to that on gas-line.

The operators like the change because the engines are much smoother than before, and because the low-speed acceleration is improved by better fuel-air ratios—even at idling speeds.

The main storage tank is located in a portion of the plant tract that is out of the way of any contemplated expansion of the buildings. A trailer-mounted 500-gal tank is used to take the working supply to a centrally located spot convenient for filling the vehicle tanks.

Savings in fuel cost have not been spectacular at this plant because of the higher delivered cost of the fuel compared with the price in the Waukegan area. In all other respects the Manville operation seems to be repeating the history of the Waukegan fleet.

Indoor vehicles have been doing so much better on propane that the company decided to convert its fleet of over-the-road trucks that handle local and yard transportation.

More recently the Watson plant at Long Beach, completed the conversion of its fleet of fork lifts and dump trucks. Fuel supply was not a problem, as there was a large propane standby plant in use at the plant. It was only necessary to connect this up with a dispensing pump located near the main factory buildings. Conversion was completed in November, 1956, and up to the time of this writing, no service calls have been necessary. It is still too early to draw many conclusions, but there is a definite reduction in fuel costs.

(Resume Reading on Page 42)

DISTRIBUTION AGE

Overhead . . .

(Continued from Page 38)

chinery has been carried by heavy-duty tractor-trailers.

Three boltmakers, each weighing more than 30 tons, were moved in four days. This period included dismantling and return to production.

Shipping Cap Screws

Shipments of cap screws to five branch warehouses and to customers are made by rail and tractor-trailer. Most shipments are made by truck while rail carloads are shipped to a west coast warehouse. Emergency shipments of cap screws—special sizes or standard sizes—in small quantities can be made with the use of a helicopter. Flights are made from a special helicopter field which has been built at the rear of the plant. Since rates are rather high, the company feels helicopter shipments will be rare, and regards them as a special service to customers.

Cleveland Cap Screw uses a number of various types of containers to ship finished products, including wooden kegs, corrugated board cartons and wood boxes.

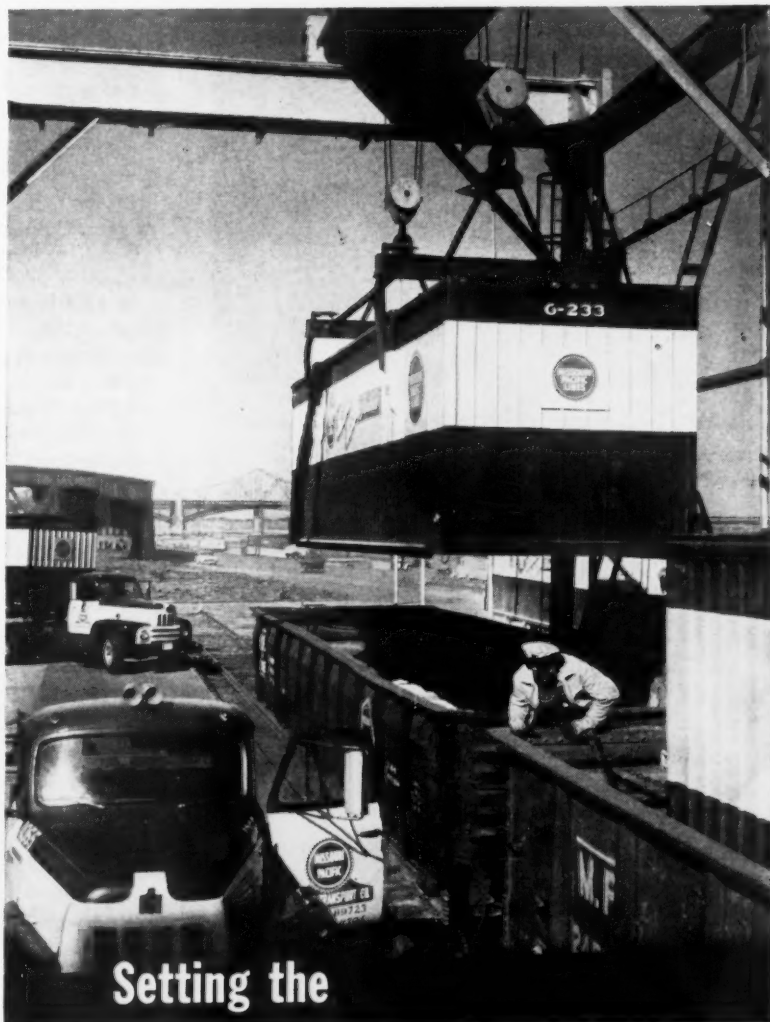
At the shipping docks, there is room enough for six tractor-trailers to park at one time. Docks are equipped with hydraulic floor levelers.

The use of modern materials handling equipment has enabled us to ship finished goods in half of the time it formerly took. We can now utilize the services of 12 employees in one area as compared with operations at the old plant where we had two widely separated shipping docks where only two trucks could be loaded at each point at one time.

We are getting maximum services from our employees in addition to increasing shipments, and handling a greater amount of cap screws more efficiently than before. At the same time, we are establishing a pattern of traffic flow which ultimately builds substantial savings for our company and our customers. •

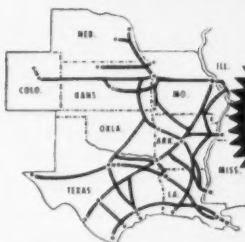
(Resume Reading on Page 39)

The wheels don't go . . . in Mo-Pac's new twist on *Truck-Rail* service. Shipping only the trailer body saves precious minutes and dollars. It's Mo-Pac . . . it's modern.



Setting the

MODERN PACE in transportation



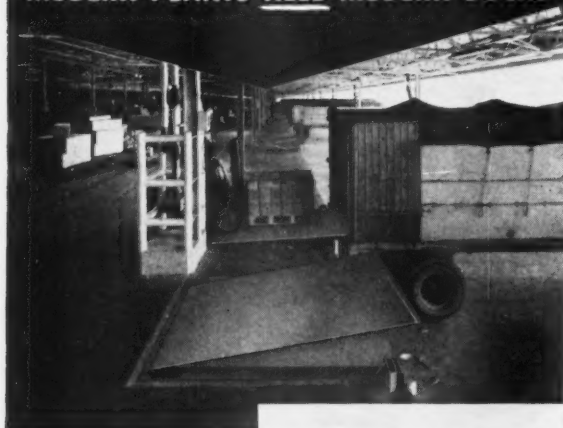
Route of the EAGLES



P. S.: We also provide regular piggy-back service!



MODERN PLANTS NEED MODERN DOCKS



*Plan
Ahead
with*



48 HI-LO Automatic Dockboards at
Ford Motor Co. Pool Car Terminal
Detroit, Michigan

HI-LO[®] FULLY AUTOMATIC DOCKBOARDS

COMPLETELY AUTOMATIC

No dock attendant needed! The carrier automatically adjusts the HI-LO to its bed level the instant it contacts the dock.

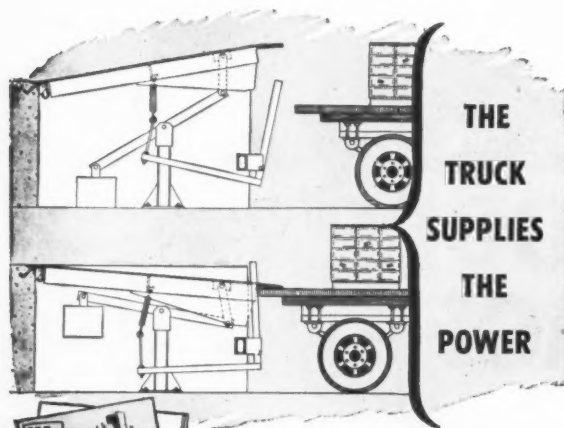
FIRST COST IS ONLY COST

Simple counterweight system. No air, electric, or hydraulic power used. Built to outlive the dock!

PACKAGED OR RECESSED MODELS

Available in lengths to solve all loading problems for new installations or for the modernization of existing facilities.

AUTOMATIC CROSS TRAFFIC LOCK WITH SAFETY STOP PERMITS TRAFFIC ACROSS WIDTH OF RAMP WHEN HI-LO IS NOT IN USE.



Send for this NEW CATALOG and BLUEPRINT FILE. It's jam-packed with pertinent information concerning the problems of dock loading and automatic dockboards.

THE KELLEY COMPANY, INC.

316 E. Silver Spring Drive • Room D • Milwaukee 17, Wis.

Circle No. 17 on Card, Facing Page 51, for more information

Port of Tacoma . . .

(Continued from Page 43)

leled by two depressed tracks. There is berthage for two ocean-going vessels in the slip, and one on the face of the pier.

Outside the Port of Tacoma, the city has a number of private facilities. Shaffer Terminals has two berths, a 48,000-sq ft transit shed, plus open storage of 70,000 sq ft served by a 15-ton movable crane. Baker Dock has two berths and a 70,000-sq ft transit shed. The Eureka Dock has a 35,000-sq ft transit shed and one berth. The Pudget Sound Freight Lines location includes a 175,000-sq ft transit shed and three berths.

In addition to these, 14 waterfront industrial plants have berthing capacity for 18 ships. All of the terminals are equipped with shipside and depressed rear tracks, motor truck tailgate platforms, and other modern cargo-handling equipment.

In addition to oil companies, industrial plants have deep water facilities to accommodate 20 ships.

Transportation

Rail service with terminal facilities is furnished by four transcontinental roads; the Chicago, Milwaukee, St. Paul and Pacific, Great Northern, Northern Pacific, and the Union Pacific. Direct service also is provided to regions north and south.

The Municipal Belt Line Railroad provides terminal switching service for all of the railroads. There are no switching charges on line-haul traffic.

More than 50 regular route motor freight carriers serve Tacoma. Freight may be shipped to and from virtually any point in the United States and Canada on a through bill of lading. There also are sufficient specialized and irregular route carriers to efficiently handle all possible requirements.

Air Service is provided within a half hour's drive from the Port at the Seattle-Tacoma International Airport. It is the terminus of seven major airlines flying to Hawaii, Australia, the Orient, Philippines, Alaska and Canada as well as all parts of the United States. Up to 110 scheduled flights daily operate through this big terminal. To meet increased air freight requirements now totaling 17 million pounds a month, new freight terminals recently have been provided.

Fifty-five steamship lines furnish foreign, coastal, intercoastal and insular service to Tacoma. Several boat lines provide service to points on the Sound. Tacoma is served by three tug and two tow boat companies providing barge and towing service between Tacoma, Pudget Sound and Alaskan points, as well as ocean service.

Public warehouse facilities in Tacoma are ample. The DISTRIBUTION AGE Warehouse Directory lists 15 such establishments. These warehouses offer storage space and services in the general merchandise, refrigerated, and household goods fields. •

(Resume Reading on Page 44)

Packaging Tied to . . .

(Continued from Page 58)

for protection of the instrument in the plant.

Shelly reported that his division is using a large telescope box on a pallet to palletize its large-volume outbound shipments. Sixteen of these units, making up a 20,000-lb. truckload, can be loaded by one man and lift truck in about 45 minutes, replacing 44 cartons formerly used to pack the same material.

Tray packs are used for small parts to eliminate handling of individual containers and reduce the number of sizes of containers. The number of handlings required per unit has been reduced from five to one, the number of cartons per truck load from 400 to 16, the time required for loading from four man-hours to 45 minutes; the labels required from 400 to 16, and the number of sizes from about 25 to 8. Storage capacity has increased by 100 per cent; freight rates have decreased; and claims have been virtually eliminated.

Love outlined savings being obtained from automatic packaging machinery and from use of wire-bound pallets. Because of palletizing, he reported, the shipping department no longer is a bottleneck. Fewer man-hours are needed to pack shipping containers; the cubic displacement in the containers has been reduced, with indirect savings in payload weight; and fewer cartons are needed—950 in January, 1957, compared to 6,500 in January, 1956.

Exposition

Principal trends in the packaging industry were reflected by the Exposition's more than 375 displays.

Plastics, both for film and for solid containers, were shown in greater variety than ever before. Materials included nylon, vinyl, polyethylene, and polystyrene as well as the conventional cellophane.

A sharp rise in the use of cellophane in the next few years, particularly in the baked goods and frozen foods fields, was predicted at the show by George R. Johnson, Film Division, Olin Mathieson Chemical Corp. Only the limited supply of cellophane has prevented its wider use, he said.

Color is another trend. Glassware appeared in more colors, sizes, and shapes. Plastic bottles were shown in pastel, bright, and metallic colors, some of them so glossy as to appear iridescent. More use was exhibited of multi-color process work on plastic films, paper, plastic boxes and bottles, and cartons.

Packages that do everything for the consumer except finance the purchase reflected the increasing stress on convenience. A package that doesn't even have to be opened is the packet of water soluble poly vinyl alcohol film shown by one firm.

No judgment is required to use the self-metering containers displayed by another firm. A one-piece folding carton will automatically measure any dry free-flowing product while pouring. Its ease of operation was demonstrated at the show by a monkey. •

(Resume Reading on Page 59)

BARRETT SINGLE-STROKE J-R LIFT-TRUCKS



**Mac says: "They load faster
—handle easier
—last longer!"**

—that's what makes the Barrett Model J-R one of the most popular lift-trucks of all time! With Barrett's patented compound lift mechanism, it takes only one easy sweep of the handle and the skid load is on its way.

Fast loading...easy anti-friction travel...on-a-dime maneuvering...help you cut handling time and effort to the minimum. And Barrett's hydraulic release check gives you safe, cushioned lowering without danger to operator or materials.

Model J-R is built for the long haul...for years of dependable service...for lowest repair bills. Yet you pay no premium for its extra quality. No comparable lift-truck offers more value—or undersells the Barrett J-R. Capacities range from 2500 to 4000 lb. For complete details, use the handy coupon.

BARRETT

ONE MAN DOES MORE THAN 3 OR 4
—WITH A BARRETT

BARRETT-CRAVENS COMPANY

604 Dundee Road, Northbrook, Ill.

Representatives in all principal cities.

Canadian licensee, S. A. Armstrong, Ltd.,
Toronto, Canada.



Please send Bulletin 555-1 on the Model J-R
Single-Stroke Lift-Truck.

Name.....Title.....

Company.....

Address.....

City.....Zone.....State.....

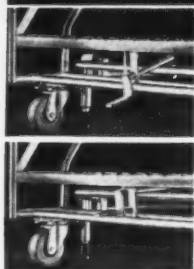
Circle No. 18 on Card, Facing Page 51, for more information

WORK AT HIGH LEVELS SAFELY...

ON BALLYMORE

"Hi-boy" SAFETY-
STEP
LADDERS

FROM 9 TO 12 STEPS



FOOT-OPERATED BALLYLOCK

Step-on brake casters on front of ladder grip floor . . . prevent ladder from rolling, wobbling or "kicking out." When pedal is released, Ballylock quickly retracts by spring action.

Strong, safe, and easily moved about, Ballymore "Hi-boy" Step Ladders provide sure support at extra-high working levels—up to 15 feet!

All-welded steel construction . . . rust-resistant aluminum finish. Smooth-rolling casters provide complete mobility. Rugged step-on Ballylock extends front legs for firm, stable floor contact.

Four sizes, six models to choose from—each with sturdy handrails. Three types of non-slip tread construction.

Write today for complete specifications to Ballymore Company, West Chester 24, Pa.

BALLYMORE LADDERS

Circle 20 on Card Facing Page 51



Traffic Management . . .

(Continued from Page 35)

Division, and filing claims for overcharge, loss and damage, and reparations.

Personnel & Executive

Our contact with the Personnel Department is limited to passenger transportation and transportation of household effects. We are called on by the Executive Department to help solve plant location problems, draw siding and other carrier agreements, and assist in production planning from the transportation standpoint.

Sales

The Sales Department is the originator of all business transactions connected with commercial distribution. All shipments of merchandise sold originate with the signing of a sales contract or purchase order.

For this reason, it is particu-

larly important to service the Sales Department with traffic and transportation requirements. Customer good will and dollar sales volume are at stake.

In performing these services properly, Traffic can consistently refine and improve the techniques and procedures so that costs can be controlled and reduced as opportunities arise.

These cost controls can benefit sales with an ultimately lower delivery price, which in turn aids the salesman in closing orders.

In cooperation with the Sales Department, it is well to recognize that no sale is complete, or customer properly served, until the supplier has "delivered the goods."

We have found that close attention to the following phases of everyday traffic activity can accomplish these objectives:

1. Furnishing information to aid

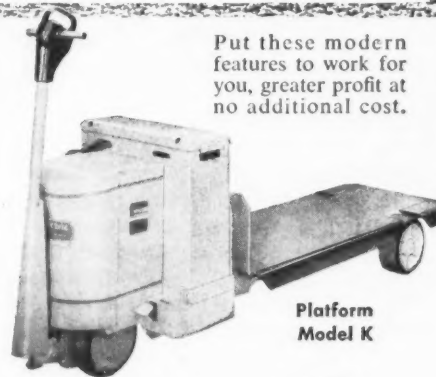
A modern truck for modern plants

The HydroLectric

with most modern features

- 2 Drive Wheels instead of one
- 2 Braking Systems available instead of one
- 4 Wheel Stability instead of three
- Alloy Gear Transmission instead of chains
- 100% more Steering Ease —Greater Maneuverability
- 50% less Maintenance —Easy Accessibility
- Finger-Tip Control —Greater Safety

Put these modern features to work for you, greater profit at no additional cost.



Platform
Model K

Write for Bulletin KW

STUEBING Designed • Engineered • Built

LIFT TRUCKS, INC. / CINCINNATI 14, OHIO



THERE IS A TRUCK FOR EVERY PURPOSE TO HANDLE ANY KIND OF MATERIAL.
Circle No. 21 on Card, Facing Page 51, for more information

DISTRIBUTION AGE

in developing areas with favorable freight rates.

2. Quoting rates to determine most desirable units of sale.

3. Furnishing information on competitors' freight rates.

4. Furnishing rates, routes, class and probable delivery dates on outbound and inbound shipments.

5. Furnishing information to aid in market surveys.

6. Building good will through service to customers.

7. Tracing, expediting, and re-consigning shipments and other such service in transit.

8. Securing rate adjustments.

9. Establishing and obtaining exception rates for benefit of the customer.

10. Developing pool-car arrangements.

11. Assuring prompt arrival of shipment at a cost consistent with what the traffic will bear.

12. Developing proper packaging and type of package insuring arrival in an attractive container and in good condition, immediately ready for the purpose for which it was ordered.

13. Prompt location and control of shipment enroute.

14. Selection of an efficient distribution pattern through strategically chosen warehouse points for prompt service from nearby stocks at more favorable combination freight rates to customer.

15. Delivery in a state and on a vehicle most adapted to the customer's receiving and unloading facilities.

Sales Team

We feel that all departments, including Traffic, make up the team behind the salesman. When the salesman closes an order, it is our responsibility to complete the distribution cycle—quickly and economically.

All steps in the movement of goods, from raw material to finished product, are the concern of Traffic. In its simplest form the one-man shipping department wraps a package, pushes it on a hand truck to the curb, and delivers it to the carrier. In its more highly developed state, the Traffic Department is that part of the management team responsible for the physical distribution of goods. •

(Resume Reading on Page 36)

NKP Piggyback helps packager cut his inventory

Pleased by our on-time, overnight service for pulpboard and paper from Chicago, a Cleveland package manufacturer recently decided to use NKP Piggyback regularly as a means of lowering his inventory. He saves space and handling, too.

He knows NKP Piggyback can deliver the next day, at his dock, whatever raw materials he needs.

More Piggyback advantages: shipper and consignee always know exact location of shipment,

rail safety, speed, competitive rates and service on all traffic and between all points where Piggyback service is maintained.

Can Piggyback help you solve a shipping or inventory problem? Phone your local NKP representative today for full information.



General Offices:
Nickel Plate Road
Terminal Tower
Cleveland 1, Ohio
Phone: MAin 1-9000



CUT YOUR distribution COSTS profitably!

COOKE *personalized* service cuts your costs, raises your profits through *individual* solutions to your particular warehousing, transportation and physical distribution problems. 30 years a leader in public merchandise warehousing in the world's greatest distribution center! Unsurpassed in specially-trained personnel, methods, equipment and facilities! Write today for detailed brochure.

when time and money count, count on



J. LEO COOKE WAREHOUSE CORP.

JERSEY CITY & TRENTON, N. J.

PENN RR: 140 Bay St. Jersey City 2
Oldfield 3-5080 • Whitehall 3-5090
Whitehead Rd., Trenton 9 • JUniper 7-4646
ERIE RR: 12th & Provost St., Jersey City 2
Oldfield 3-5080 • Whitehall 3-5090
TWX: JC-112



EASTERN REPRESENTATIVES FOR:
North Pier Terminal, Chicago, Ill.
Encinal Terminals, Alameda, Calif.



Palletized Brewery . . .

(Continued from Page 53)

The problem was solved by designing and building a unit with combined acceleration principles of live rollers with low acceleration pressures of gravity conveyor. Storage conveyors were given a slight slope of approximately $\frac{3}{8}$ in. per linear foot. Slight drive pressures were maintained by making the unit a live roller conveyor with driven rollers located at 18-in. centers instead of the usual 6-in. centers.

As an added precaution, metering belts are used to split storage conveyors into two distinct units.

Automatic Palletizing

The palletizer is a combination of photoelectric cells, counters, air operated mechanisms, motors and limit switches.

As pallet loads of ale or beer accumulate on the upper two storage lines, they are run automatically through the palletizer and

discharged in pallet loads of 125 dozen pints each.

No pause in operation is experienced if one line is running faster than the other. The palletizer responds only when full pallet loads have accumulated. This makes it possible to draw two pallet loads from one line while only taking one from the other without attention from the operator.

Empty pallets are fed onto the transfer platform from an empty pallet magazine holding 11 pallets. They are elevated one at a time to the top of the machine. During the elevation period, a layer of 10 two-dozen pint cartons is built up on a steel plate which extends over the pallet. The plate is withdrawn and the cartons are deposited on the pallet. The pallet then is lowered the equivalent of the height of one carton while another layer is built up on the steel plate.

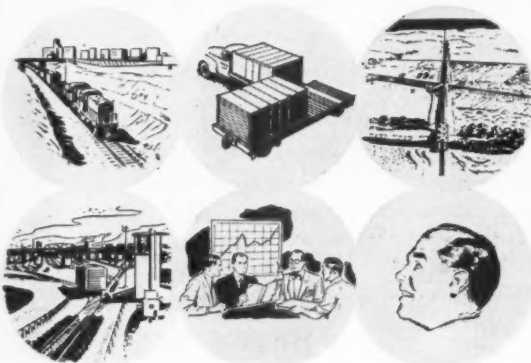
The pattern used for placing alternate layers is changed automatically so that cartons are interlocked from layer to layer. The second tier is dropped on the first layer and the process is repeated until six layers of cartons have been placed on the pallet. The loaded pallet then is discharged to a fork truck station for transportation to the storage area or direct to the loading platform.

From storage, on a first-in-first-out basis, full cartons are taken by fork truck to assembly areas adjacent to truck loading bays. Low-lift trucks take over pallet loads here and carry them over the hydraulic ramps into the waiting motor trucks.

When empties are returned to the plant, the cycle is reversed. Low lift trucks are used to remove empties from trucks and carry them into the assembly area. Fork trucks take over pallet loads here and carry them on to storage. In storage they are headed via conveyor system to the washing department. •

(Resume Reading on Page 54)

proven formula for a Satisfying Shipping Service



Ample diesel power—up-to-date rolling stock—well-conditioned roadbed—time saving classification yards—modern methods and facilities—a trained, eager-to-serve personnel . . .

—this is a Rock Island Formula!

For detailed information consult any Rock Island representative.



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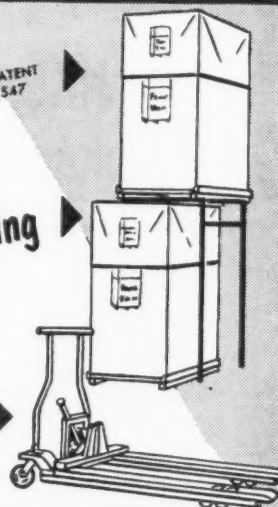
The Road of Planned Progress—
Geared to the Nation's Future

WITH OUR

Pallet Vault ^{U. S. PATENT 2,709,547}

Single or
Double Stacking
and

General
Jack Lift



You've got the cleanest . . . most efficient
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Inquiries Invited!

PALLET VAULT CORP.

4908 DELMAR BLVD. • ST. LOUIS 8, MO.

Write For Address of
Nearest Agent

New Labor Outlook . . .

(Continued from Page 55)

in social activities scheduled at that time.

Operational seminars and clinics, restricted to membership of the divisions were held Thursday.

Merchandise Division seminars and their leaders were: Office Procedures, Stern, H. C. Dickelman, and Lloyd E. Long; Legal Aspects of Warehousing, Schall and Butler; Business Promotion, M. Wistar Wood, dean, Charles Morris Price School of Advertising and Journalism, Philadelphia, W. C. Hudlow, Jr., and Capron; Traffic, Paul Schwartz; and Port Terminals, Bruce S. Howard.

Also, Operating Ratios, C. B. Taylor, Frank P. Ellis, Jr., Greeley, and John Doggett; Physical Operations, C. J. Heinrich, U. S. Naval Supply Research and Development Facility, Bayonne, N. J., and A. M. Lownsbury; Labor Relations, E. J. O'Keefe, Chicago, and Dozier; Contract Terms and Conditions, Harold M. Willenson; and Government Relations, F. D. Bateman.

Leaders of the NARW Pin-Pointer Clinics were: V. C. Stevens, Operations and Handling, Break-up Space, Modernization; W. C. Hudlow, Jr., Business Promotion, Adding New Services; W. C. Baker, Office Procedures and Forms Bases for Tariff Construction, Taxes; and A. W. Oakley, Jr., Loading Docks, Loading Time, Truck Demurrage, Temperatures and Commodities.

Preceding the clinics on Thursday, the NARW heard a talk on "Let's Analyze Our Planning," by A. N. Otis, of New York.

Two panels on profits also were on the program. "Planning for Profits—In Black and White" was moderated by C. A. Bowen, Jr. Panel members were Otis, H. Farnsworth, G. F. Dodson, A. R. Current, John L. Bear, L. M. Ashenbrenner, and H. C. Diehl.

"Profits Can Be Promoted" was a panel moderated by G. A. Shoemaker. Others taking part were Hubert Arons, H. J. Nissen, L. L. Baker, W. B. Haggarty, F. D. Newell, Jr., David B. Stearns,

D. H. Murphy, and H. L. Brooks.

On the closing day, there were both division and general meetings. The Merchandise Division summed up its Operational Seminars in a Town Meeting of the Industry. A panel appearing before the NARW covered the subject, "Let's Get Our Costs Down." Taking part in the panel were William E. Ready, C. E. Adams, Carl E. Johnson, Alex MacTaggart, Arthur Weyhe, William H. Volkmann, and William Giacomazzi, Jr.

First timer breakfasts were held by the NARW on Tuesday and the Merchandise Division on Wednesday. The speaker at the Tuesday event was Glenn B. Sanberg, executive vice president, American Society of Association Executives. He spoke on "You—Unlimited." The President's Reception and 66th Annual Banquet were held on Thursday night. •

(Resume Reading on Page 56)

Circle 23 on Card Facing Page 51

REVOLVATOR GO-GETTER
The MOST COPIED LIFT TRUCK On The Market

EASIEST WALKIE-TRUCK TO OPERATE AND MAINTAIN EVER!

FINGER-TIP CONTROLS INCLUDING DYNAMIC BRAKE TO ELIMINATE "PLUGGING" THE MOTOR GOING DOWN RAMP, AND "ARTICULATED" CONSTRUCTION ADJUSTS TO UNEVEN FLOORS. STEPS EASILY AND WORKS IN LESS SPACE BECAUSE OF THE DUAL WHEELS & DIFFERENTIAL DRIVE!

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NOW! FACTORY OR FIELD INSTALLATION KITS for 3000 to 20,000 lb. Trucks

Swing-Shift speeds carloading up to 28%. Less damage to loads and fragile merchandise.

SWINGS and SHIFTS loads quickly into exact alignment without moving truck from original approach.

Operator SWINGS and SHIFTS loads (forks) while truck is moving—eliminates all lost motion.

Swing-Shift MFG. CO.
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See Your Lift Truck Dealer or Write—

Circle No. 24 on Card, Facing Page 51, for more information

Within the



By Leo T. Parker Legal Consultant, Distribution Age

TRANSPORTATION

Is common carrier liable for injury or loss of livestock without proof of negligence?

In the past the writer received numerous inquiries from officials and employes of transportation companies regarding liabilities for negligence when transporting livestock and other merchandise. A few weeks ago a higher court rendered a decision to the effect that a common carrier may be liable for injuries to and loss of shipped livestock without proof that the carrier was negligent.

For example, in *Y— Trucking Co. v. G—*, 281 Pac. (2d) 1004, the higher court held a common carrier liable for all damages and injuries to shipped livestock because it breached its contract to safely transport the livestock. The testimony showed facts, as follows: One G— employed the Y— Trucking Co. to transport cattle from Colorado to a definite loading area in Wyoming. The carrier loaded thirteen trucks with 228 cows and 242 calves for transport along a specified route. In Wyoming, because of rain, the trucks mired in the dirt road and none of them reached their destination point. At Tie Siding, Wyo., G—'s agent met the trucks; and designated the remaining route to be followed.

In subsequent litigation G— contended that the carrier was negligent; that the cattle were unduly delayed in transit without water and feed; were roughly handled; and many wounded, bruised and thrown down and injured. Finally the cattle were "jumped" along the route, from the trucks by the drivers, many in unfenced country where the cows and calves separated and roamed for long distances. Some 22 cows and 44 calves died in the trucks, were compensated for by G—'s insurance company, and are not involved in this controversy although later the insurance company, which paid G—, may sue the carrier to recover money paid to G—.

The testimony showed further that after some six weeks, 40 cows and 70 calves of the "jumped" cattle remained unaccounted for, some having died or been permanently lost, cows aborted and calves "bummed", and others had lost materially in weight. It was stipulated the value of a deceased cow was \$175 and a deceased calf \$55.

The carrier denied it violated its contract of carriage, denied that it was negligent, alleged that any damage was due to an act of God or to acts of G—'s agent.

The jury rendered a verdict in favor of G—. The carrier appealed to the higher court and urged the following points for reversal: (1) It was incumbent on G— to prove specific negligence of the carrier. (2) That G— and his agents committed negligence and (3) That G—'s employes assumed control of the shipment which would relieve the carrier of liability. (4) That the damages awarded were speculative

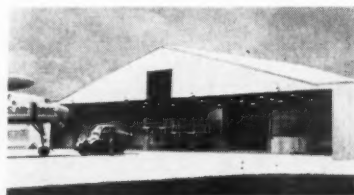
and uncertain. In other words, the carrier's points for reversal may be combined into its two defenses: (a) That the damages were due to an act of G—'s agent; (b) or due to an act of God.

It is interesting to observe that the higher court held the carrier liable for all damages to the livestock, and said:

"The general rule maintained by all the authorities is that carriers of live stock are liable absolutely for loss of or injury to stock intrusted to them for transportation, like other common carriers, unless the loss or injuries were occasioned by the act of God, or the public enemy, or the negligence of the shipper, except that they are not liable for loss or injury caused by natural propensities of the animals themselves. The present action was for breach of contract. Plaintiff was not required to establish negligence on the part of defendant, but only to establish the breaches of the contract. This being shown, the resulting damages therefrom would be recoverable independently of the question of defendant's (carrier's) negligence."

Therefore, in this case the Y— Trucking Co. was held liable for all damages to the shipped livestock because it breached its contract to safely transport the cattle.

Air Maintenance Dock



New maintenance dock designed for U. S. Air Force to service seven types of medium-sized bombing and transport aircraft. Standardized steel dock covers wing, engine and nose portions of aircraft, while tail and part of fuselage extend outside. Weather-tight closure is provided by sliding doors and, at center, fabric curtains that fit tightly around protruding fuselage of plane. Dock comes equipped with specially-designed work platforms that fit around aircraft. Luria Engineering Co. has constructed 69 docks at AF bases throughout the world under initial contract

In determining pick-up charges, is a sheltered loading dock considered interior or exterior?

In *F— T— Lines v. R—*, 133 N. E. (2d) 699, testimony showed facts, as follows:

The wording of the filed tariff schedule provides that when a carrier performs pickup or delivery service to or from the interior of a building, the lawful charge is 25¢ per 100 lb. At all other points the charge is 11¢ per 100 lb. For two and one-half years the F— T— Lines performed pickup and delivery service in transporting goods from a warehouse to other cities in the state.

After business relations with the

warehouse company ended, F— T— Lines brought suit to recover additional freight charges upon the basis that the charges which had been paid were less than those specified by the motor freight tariffs on file with the Public Service Commission.

The additional charges claimed to be owing are based on the theory that it was necessary for F— to pick up this merchandise from the interior of warehouse building at points inaccessible to motor vehicles.

In other words, the F— T— Lines contended that for two and one-half years it was paid 11 cents per 100 lb for transported merchandise when, in fact, it should have been paid 25 cents per 100 lb because the tariff provided this latter charge for merchandise picked up from the interior of the warehouse.

Testimony showed that the warehouse loading platform is sheltered from the weather, being recessed partly beneath a portion of warehouse. The loading platform projects out from the wall of the building. Seven large overhead doors open through the wall of the building along the inner edge of this platform. When these doors are open, as they are when pickups are made, they reveal that this platform continues inside of the wall of the building on the same plane for an additional width of 48 ft.

When merchandise was ready to be shipped, the practice was for the warehouse employees to carry it from the storage part of the warehouse to the bays on the interior portion of the loading platform. All items consigned to one city were placed in a single bay over which was suspended a sign bearing the name of that city. These items thus waited to be called for where they were not exposed to the weather.

In view of this testimony the higher court held that the F— T— Lines was not entitled to collect transportation charges based upon tariff charges for goods taken by the carrier's employees from the interior of the warehouse building.

Can employe recover damages for injury received riding his employer's truck after hours?

Recently a higher court held that an employe who sustains an injury while riding in his employer's motor van cannot recover damages for the injury, if testimony shows that the employe was not acting within the regular scope of the employment when the injury was sustained.

In L— v. H— Co., 293 Pac. (2d) 348, it was shown that an employe of the H— Co. was severely injured when he was riding in his employer's motor truck. L— sued his employer, the H— Co., for heavy damages.

Testimony showed that L—, after close of working hours, requested a ride on his employer's truck to his employer's other place of business in a direction opposite from his home.

In view of this testimony the higher court refused to hold the H— Co. liable in damages for L—'s injuries. The court said:

"Where the employe, after the close of working hours, requested and obtained a ride on his employer's truck on a private mission of his own, injuries sustained in course of the ride did not arise out of and in course of employment."

Is tractor-trailer owner liable for driver negligence resulting in highway accident?

Recently, a higher court held the owner of a tractor-trailer liable in heavy damages for injuries to an occupant of a passenger automobile being followed too closely by a tractor-trailer. The testimony which resulted in the owner of the tractor-trailer being liable was that the latter vehicle was only 150 ft behind the passenger automobile when the accident happened.

For example, in T— v. R— J— Trucking Co., 295 Pac. (2d) 742, it was shown that one T— was an occupant of a westbound automobile. He sued the owner of a following tractor-trailer for injuries sustained in a three-vehicle collision. The higher court awarded T— heavy damages from the R— J— Trucking Co. because the testimony showed that at the time of the accident the road was snowy and slippery, and that the driver of the tractor-trailer was negligent in fol-

lowing T—'s automobile at the distance of only 150 ft.

WAREHOUSING

When furniture has been stored in many warehouses, who is held liable for damage?

A higher court rendered a decision to the effect that unless the owner of merchandise stored in various warehouses proves positively that the merchandise was damaged by a particular bailee, the owner of the damaged goods can recover no damages.

In T— v. P— Van Lines, 2287 S. W. (2d) 384, the testimony showed facts, as follows: One, T—, is an officer in the United States Navy. He was stationed in Washington, D. C., and resided in Arlington, Va. At that time he was ordered to Japan and his wife, who was not to accompany him, planned to go to Tulot, Ark., where her mother resided. They were confronted with the problem of moving their furniture from their home in Arlington and decided to store it in a warehouse of the A— Van Lines located in nearby Alexandria, Va. The furniture was called for by the driver of an A— Van Lines truck and the furniture was then taken to the A— Van Lines warehouse where it was stored.

Six months later T— employed the P— A— Van Lines to move the furniture from the warehouse in Alexandria to Tulot. When the truck arrived at Tulot it was met by T— and the driver was told to place the furniture in a vacant house.

Two months later T— returned to Tulot and employed the N— Van Lines to move the furniture to St. Louis. The driver for N— Van Lines testified that the goods were not placed in the house properly, some pieces being put on top of others, and that he directed Mrs. T—'s attention to some items of damage. He did not make an inventory of the goods as he loaded them, but delivered them to the F— Storage Co. in St. Louis. The furniture remained in the F— warehouse for three months when it was removed to a new home of the plaintiffs in Olivette, Mo.

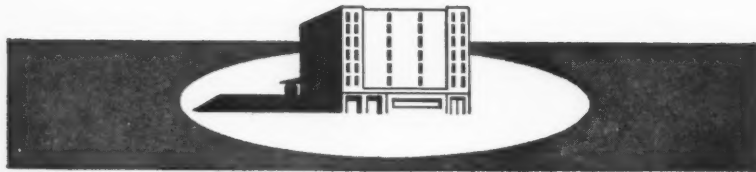
Then T— for the first time examined the furniture and determined what damage all of it had suffered. The cost of repairing the furniture was \$2,260.50. T— sued the various handlers of the furniture to recover this amount and proved that when the furniture was accepted by the warehouse company in Alexandria, Va., for storage it was in good and first class condition. It is interesting to observe that the higher court refused to allow any damages.

35 Per Cent Saving



This package, a prize winner in the recent SIPMHE Packaging Competition, is used by Greenlee Tool Co. for the shipment of hydraulic pipe benders. The user reports that the new package offers complete protection, high stacking possibilities, and a 35 per cent cost saving

Warehouse SPOTLIGHT



New Jersey Warehousemen Elect Officers, Board

The New Jersey Furniture Warehouse Association held election of officers and board members during its recent Annual Meeting.

Officers are Charles Hellrigel Jr., of Newark, president; Robert Schuenemann, of Bell Storage, first vice president; William Cumberly, of Mannings Warehouse, second vice president; Jack Muligan, Knickerbocker Storage, secretary; and Frank O'Connor, of O'Connor's Moving and Storage, treasurer.

Allied Elects Nine to Board

At a meeting in Atlantic City conducted by President John E. Flynn, Allied Distribution, Inc., approved expansion of its Board of Directors to nine.

Following this move, the below named were elected: W. D. Leet, chairman of the Board; Flynn; James Robertson, vice president; Herb Clark, Jr., Portland, Ore.; James Finn, Denver, Col.; Robert Abernathy, Sr., Dallas; F. Donald Bateman, Chicago; M. A. Brickey, Philadelphia; and Al Laney, Jacksonville.

Overmyer Program Presented to Members



The 1957 budget and a new advertising program were discussed at the Annual Meeting of D. H. Overmyer Warehouse Sales Co. held during the recent Annual Meeting of the American Warehousemen's Assn. Shown above (left to right) are: John G. Downing Jr., Kencroft Warehouse; Bert Veenker, Polar Cold Storage Co.; Ralph Rycroft, Jr., Kencroft Warehouse; Dan Overmyer, president; Leo Pistorino, Bowker Storage and Distributing Co.; Dennis Campbell, New York; George Kerwin, Toledo, Ohio; R. T. Rycroft, Sr., Kencroft Warehouse; and H. M. Overmyer, Toledo

Strobel Elected AWI President

Wallace C. Strobel, of Saginaw, Mich., was elected president of the Associated Warehouses, Inc., during its annual meeting in Atlantic City.

Other officers elected at that time were Walter F. Winters, Buffalo, N. Y., vice president; George W. DuBois, Minneapolis, Minn., treasurer; Clyde E. Phelps, Chicago, executive vice president; and A. O. Walde, of Los Angeles, chairman of the Board.

Newly elected directors are: Ward Castle, Chicago; H. C. Cockrell, Richmond, Va.; W. D. Day, Toronto, Canada; Harry C. Goble, Portland, Ore.; F. C. Hubbard, Grand Rapids, Mich.; S. J. Lusby, East St. Louis, Ill.; Leo Pistorino, Boston; and H. W. Verrall, Dayton, Ohio.

—DA—

Warehouse Briefs

Chelsea Warehouses, Inc., has purchased a four-acre plot in Englewood, N. J., for erection of a 30,000 sq ft warehouse terminal.

Newly appointed agents of Aero Mayflower Transit Co., Inc., are Arnette-Holden Transfer and Storage Co., Murfreesboro, Tenn.; Victor Storage and Moving Co., Inc., Chicago; Julka Moving and Storage Co., Fond du Lac, Wis.; Fischer Trucking Co., Washington, Mo.; Reliable Transfer and Storage, Muskogee, Okla.; and Harcastle Transfer, Mobridge, S. D.

Lyon Van Lines, Inc., recently held safety award dinners in four cities honoring drivers who rolled up a total of 12-million accident-free miles.

During the annual convention of the New Jersey Motor Truck Assn., held in Asbury Park, Eldrege Storage, of Atlantic City, received a certificate of achievement marking 70 years in vehicle transportation.

Union Warehouse Company, Charlotte, N. C., and Kencroft Warehouse Div. of Kencroft Malleable Co., Inc., Buffalo, N. Y., have joined the D. H. Overmyer Warehouse Sales Co.

New members of the American Chain of Warehouses, Inc., are National Terminals, Montreal, Canada; Hansen Storage of Madison, Inc., Madison, Wisc.; Pinter Warehouse, Inc., Deer Park, N. Y.; Merchandise Warehouse Co., Inc., Indianapolis, Ind.; and Racine Storage and Transfer Co., Racine, Wisc.

North American Van Lines, Inc., has announced the extension of service to Japan. Household goods will be handled through the U. S. ports of Long Beach, San Francisco, and Seattle.

Haslett to Head ACW

The American Chain of Warehouses, Inc., at its 46th Annual Meeting, named S. M. Haslett, Jr., of San Francisco, president.

John K. Dozier, of Houston, Tex., was elected vice president; J. C. Miller, of Little Falls, N. Y., treasurer; and John W. Terreforte, of New York, executive secretary and assistant treasurer. The sales organization also discussed plans for the coming year at the Atlantic City meeting.

—DA—

The Household Goods Carriers' Bureau has moved its office to 2000 P Street N.W., Suite 305, Washington 6, D. C.

—DA—

Men in the Spotlight

Edmund F. McGuire—appointed executive vice president and general manager of United States Van Lines, Inc., of Franklin Park, Ill.



William Moore—named a district manager of Aero Mayflower Transit Co., Inc., working out of the firm's Indianapolis, Ind., headquarters.

Virgil Young—newly appointed chief engineer at Alford Refrigerated Warehouses, Dallas, Tex. He will continue as superintendent of maintenance as well.



John J. Falbe—has joined United Van Lines as district manager for the middle eastern states.

Richard H. Powell—named general manager of Weeks Moving and Storage Corp., Rockville Centre, N. Y.

Charles W. Barth—elected vice president and director of the Sarasota (Fla.) Transfer and Storage Co.

Lewis E. Neary—vice president of Blanchard Storage Co., has been elected first vice president, Transportation Club of the Rochester Chamber of Commerce.

Cornwall Re-elected by DSI

Distribution Service, Inc., meeting during the recent AWA annual meeting, re-elected R. M. Cornwall, of Cornwall Warehouse Company, president.

Elected at the same meeting were D. E. Taylor, Taylor-Edwards Warehouse and Transfer Co., vice president; H. F. Partridge, Anchor Storage Co., treasurer; and J. G. Temple, of Chicago, secretary.

In addition to the above officers, the Board of Directors will include C. J. Grimley, Wiggin Terminals, Inc.; H. R. Bradshaw, Terminal Warehouse Co.; Jay Weil, Jr., Gulf Shipline Storage Corp.; Glover Funderburk, Zanes-Ewalt Warehouse; and S. A. Godman, United Warehouse and Terminal Corp.

—DA—

Recently elected officers of the Staten Island Van Owners Association are: President Vincent H. Schnurr, Rosebank Storage Warehouse, Inc.; Vice-President Philip Brown, Richmond Storage Warehouse; Secretary Joseph Drennan, Joseph F. Drennan and Sons; and Treasurer William Aylwerd, Irving Storage Warehouse.

Canadian Warehousemen Announce Convention Plans

The Canadian Warehousemen's Association and its three divisions will meet May 13-16 at Harrison Hot Springs, B. C.

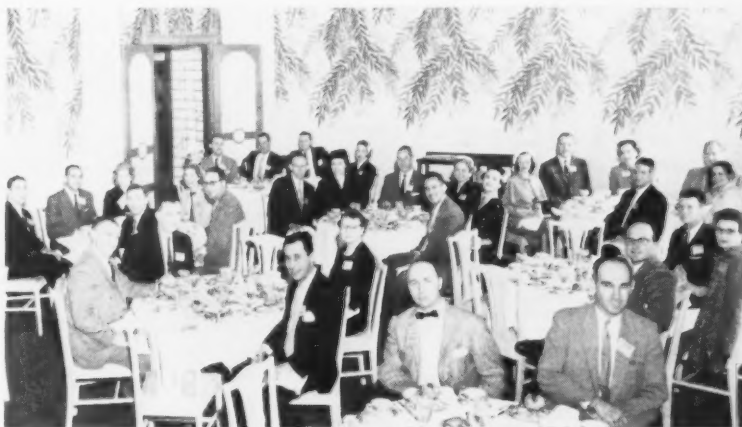
E. P. Carr, CWA president, in announcing the plans revealed that representative customers have been asked to participate in a Consumer's Panel. Through this it is hoped that delegates will learn what customers look for and what problems they experience.

The three divisions are planning programs under the leadership of J. M. Atwell, Household Goods Moving and Storage Division; R. K. Arkley, Merchandise Storage Division, and J. R. A. Langs, Refrigerated Storage Division.

—DA—

A new nationwide air-van service for household goods has been announced in a joint statement by North American Van Lines, Inc., and United Air Lines. North American makes the pickup and delivery of containerized shipments packed by its personnel. United then carries the containers on its all-cargo DC-6A's.

Affiliated Group Holds Annual Meeting



The Fourth Annual Meeting of Affiliated Warehouse Companies was held during the recent Annual Meeting of the AWA. Members and their wives shown above are: M. H. Burman, Mr. and Mrs. George Leno, Ralph Nogg, Tom Seeman, Sam Hann, Mr. and Mrs. John Downey, Mr. and Mrs. R. J. Lamneck, Sheldon Ackerman, Norman Levitt, Mr. and Mrs. Charles Duggan, Mr. and Mrs. Robert Brooks, Mrs. E. Boyd Watkins, Mr. and Mrs. Harold Drury, David Stearns, Mr. and Mrs. C. W. Hebert, Mr. and Mrs. Walter P. Taylor, V. C. Stevens, Mr. and Mrs. Malcolm McConnell, Harry Rothstein, Mr. and Mrs. Harry Gardner, and Mr. and Mrs. Herman Faure.

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Operating in San Francisco, Oakland, Stockton and Sacramento

Member: American Warehousemen's Assn.
American Chain of Warehouses, Inc.

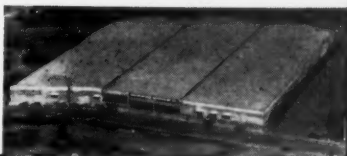
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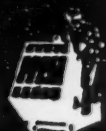
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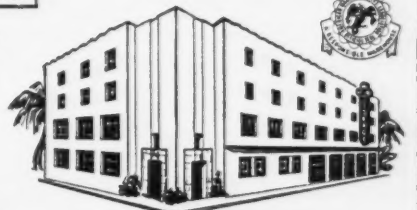
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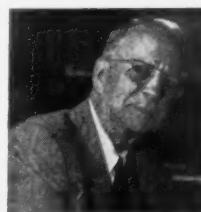
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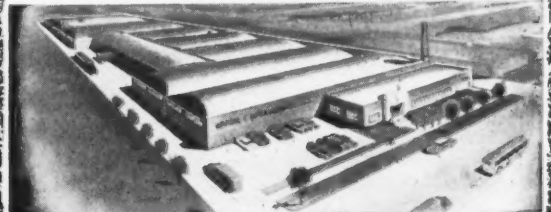
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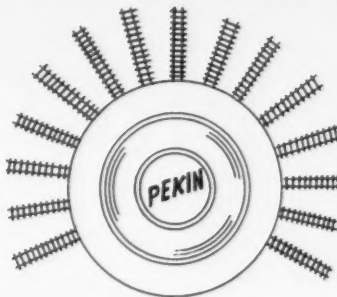


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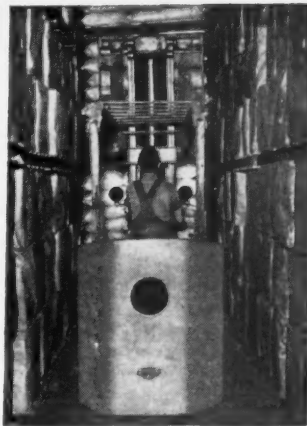
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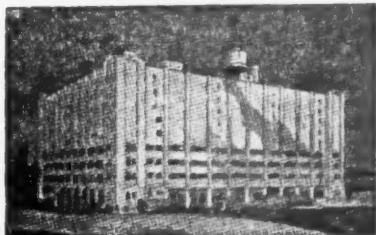
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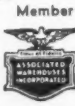
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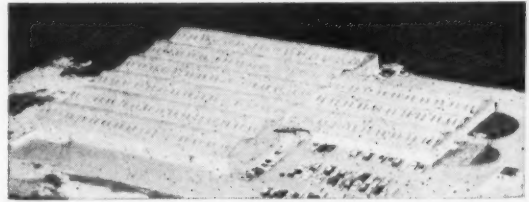
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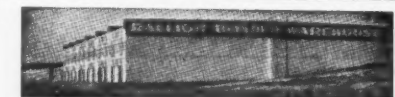
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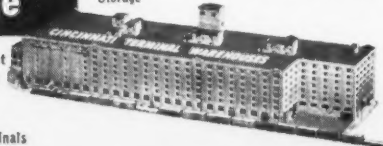
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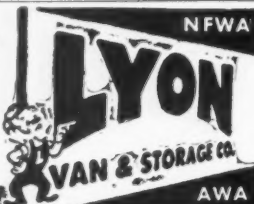
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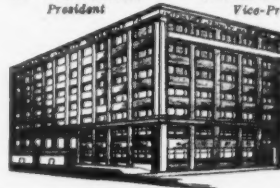
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STORAGE & VAN CO.**
901 W. VICKERY Edition 3-5301
FORT WORTH, TEXAS



and Firms are Arranged Alphabetically

NOW... IN FORT WORTH
COMPLETE WAREHOUSING FACILITIES
NEW MODERN ONE STORY BUILDING
 Completed June '55



UNITED STATES COLD STORAGE

- MERCHANDISE STORAGE
- AIR CONDITIONED STORAGE
- COLD STORAGE

1101 N. E. 23rd St. FORT WORTH MARKET 6-8246

HOUSTON, TEXAS "Service with Reliance"

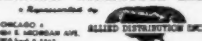
AMERICAN WAREHOUSES, INC.

P. O. Box 1772 • 1918 Collingsworth • Phone: Capitol 8-4381
 One Floor—Car Level—330,000 Square Feet—Fully Sprinklered. A.D.T.—
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 This modern one-story property with high ceilings and unlimited floor load capacity is fully equipped with modern materials handling apparatus.

HOUSTON CENTRAL WAREHOUSE and COLD STORAGE CO.

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General Storage Cold Storage U. S. Customs Bonded
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And For Pool Car Distribution

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 Pool Car Distribution
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TEXAS SERVICE WAREHOUSE COMPANY
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Private Rail Sidings—All Houston Railroads
 General Merchandise—Storage and Distribution
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SPRINKLERED THROUGHOUT—SUPERVISED BY A.D.T.
 MOST MODERN FACILITIES IN SOUTHWEST
 OFFICES YEAR AROUND AIR CONDITIONED
 MERCHANDISE STORAGE EXCLUSIVELY
 A FISHER G. DORSEY INTEREST

HOUSTON, TEXAS **UNIVERSAL TERMINAL WAREHOUSE CO.**
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Merchandise Storage—Pool Car Distribution—Drayage Service
 A.D.T. Central Station Automatic Supervisory
 Sprinkler, Waterflow, and Fire Alarm Service
 Watchmen, U. S. Customs Bonded, Office Space
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 Division of
UNITED STATES FREIGHT CO.
 Members State and Local Associations

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WALD TERMINAL WAREHOUSE CO., Inc.
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 Established 1914

MERCHANDISE STORAGE • POOL CAR DISTRIBUTION
 Represented by: *Distribution Service, Inc.*
 New York—Chicago—San Francisco
 EXPERIENCE • SERVICE • RESPONSIBILITY

HOUSTON, TEXAS BENJ. S. HURWITZ, Pres.

WESTHEIMER
Transfer and Storage Co., Inc.
 2205 McKinney Ave., Houston 1
 Since 1893

Merchandise & Household Goods Storage—Pool Car Distribution—
 Lift Van Service—20 car lengths of trackage
 Warehouses—A.D.T. Automatic Fire and Burglary Protection
 Members N.F.W.A. State and Local Assn.
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POOL CAR DISTRIBUTION
 Since 1920

Spot stocks of Grocer Lines a Specialty

TARRY WAREHOUSE & STORAGE CO.
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Members: SWATA—NFWA—AVL

SALT LAKE CITY, UTAH

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 520 West 2nd South St., Salt Lake City 1

MERCHANDISE STORAGE
 Concrete Sprinklered A.D.T.
 Pool Car Distribution Office Facilities
 Member A.W.A.—Amer. Chain of Whses.

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40,000 sq. ft. floor space. Low insurance rates. Mds., warehousing and distribution. Private railroad siding. Pool car distribution. Freight truck line. Household goods stored, packed, shipped. Van service to all states.

Member: UNITED VAN LINES
 NATIONAL FURNITURE WAREHOUSEMEN'S ASSN.

NORFOLK, VA. Fine Warehousing Since 1911

Security Storage and Van Co.
 500-530 FRONT STREET

COLLECTIONS • POOL CARS • DISTRIBUTION
 MOTOR VAN AND LIFT VAN SERVICE
 Member—Nat'l. F.W.A.—Allied Van Lines

NORFOLK, VA. Established 1892 Phone: MAdison 2-2619

SOUTHGATE STORAGE CO., INC.
SOUTHGATE TERMINAL
P. O. Box 840

MERCHANDISE STORAGE

- Fully Mechanized and Palletized
- Centrally Located
- Pool Car Distribution
- Private Trackage
- Custom Bonded Space
- Local Truck Delivery Service
- Negotiable Receipts

Represented By American Chain of Warehouses

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ROANOKE, VA. Agent: Allied Van Lines, Inc.

PITZER TRANSFER, STORAGE & FUEL CORP.
403-411 W. Salem Ave., Roanoke 5

Capacity 500 Cars
Private RR Siding
Automatic Sprinkler
Accurate Accounting

We make a specialty of Storage and Pool Car Distribution for Agents, Brokers and General Merchandise Houses.

Represented by: American Chain of Warehouses, Inc.

SEATTLE, WASH.

TAYLOR-EDWARDS WAREHOUSE & TRANSFER CO., INC.
1020 Fourth Avenue South Seattle 4

WAREHOUSING • DISTRIBUTION • TRUCKING

Represented By
DISTRIBUTION SERVICE, INC.
New York—Chicago—San Francisco

RICHMOND, VA. 79 Years of Continuous Service

BROOKS TRANSFER & STORAGE CO., INC.
1224 W. Broad St.

3 storage warehouses: 81,000 sq. ft. floor space. Low insurance rates. Mds. warehousing and distribution. Private railroad siding. Pool car distribution. Freight truck line. Household goods stored, packed, shipped. Van service to all states.

Member: UNITED VAN LINES NATIONAL FURNITURE WAREHOUSEMEN'S ASSO.

SPOKANE, WASH. Serving SPOKANE and the INLAND EMPIRE

RIVERSIDE WAREHOUSES, Inc.
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Member Firm
PACIFIC NATIONAL WAREHOUSES, INC.
ONE coordinated warehousing and pool-car distribution service

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OLD DOMINION STORAGE WAREHOUSE, Inc.
2502 Patterson Ave., S. W. Roanoke, Va.

GENERAL MERCHANDISE STORAGE
POOL CAR DISTRIBUTION
FIREPROOF BUILDING
LOW INSURANCE RATES
PRIVATE SIDING N. & W. R. R.
AMPLE TRUCK DOCKS
COMPLETELY PALLETIZED

The Most Logical Distribution Point for Virginia, West Virginia, North Carolina and Tennessee

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800 N. Hamilton St. Spokane 11

WAREHOUSING • DISTRIBUTION • TRUCKING

Represented By
DISTRIBUTION SERVICE, INC.
New York—Chicago—San Francisco

TACOMA, WASH. Member AWA, NFWA

PACIFIC STORAGE, Inc.
1721 JEFFERSON AVE., TACOMA 2

Complete facilities—5 locations including 2 on waterfront—special arrangements for serving Alaska—pool car distribution—local cartage—customs bonded—Agents Allied Van Lines.

YOU CAN SAVE MONEY BY USING TACOMA

Chuting the News . . .

(Continued from Page 16)

New York Chapter, AST&T, Holds General Organization Meeting

Dr. Virgil D. Cover, professor of Transportation, Syracuse University, was the principal speaker at the first general organization meeting of the New York State Chapter, American Society of Traffic and Transportation.

He outlined the educational program of the Society, pointing out the growing number of men taking the examinations of the Society.

Following the general meeting, the Board of Directors elected Bert H. Peterson, chairman, and Frank E. Asher, vice chairman of the Chapter.

Foundation Announced

An offer by Col. Thomas J. Weed, USA (Ret), to establish a \$20,000 foundation from his personal estate was accepted recently by the Executive Committee of the National Defense Transportation Association. The foundation will perpetuate the patriotic aims and objectives of the Association, it was stated by Col. Francis W. Crary, executive vice president, in announcing the decision.

—DA—

The National Wooden Box Association will meet June 24-26 at the Lake Placid Club, Essex County, N. Y.

Lake Carriers Elect

The complete slate of officers was re-elected recently by the Lake Carriers' Association at a meeting in Cleveland, Ohio.

They are: Vice Admiral Lyndon Spencer, president; Oliver T. Burnham, vice president and secretary; F. J. Hollman, treasurer; and Gilbert R. Johnson, counsel.

Elected to the board were Rear Admiral Charles R. Khoury, U. S. Steel, Pittsburgh Steamship Division; H. L. Hale, U. S. Steel, Pittsburgh Steamship Division; A. B. Cozzens, Columbia Transportation Co.; Paul R. Nichols, Wisconsin Steel Works, International Harvester Co.; and Robert Berkey, Cargo Carriers, Inc.

Two new directors selected by the stockholders were John J. Boland, Jr., American Steamship Co., and Admiral Khoury.

and Firms are Arranged Alphabetically

GREEN BAY, WIS.

ESTABLISHED 1903

LEIGHT TRANSFER & STORAGE CO.

1401-55 S. STATE ST. • GREEN BAY • WIS. •



Merchandise Storage
Pool Car Distribution
Transit Storage
Household Goods Storage
Heated—Unheated—Yard
Storage
Waterfront Facilities
Stevedore Services

U. S. Customs, State and
Public Bonded
70 Car Track Capacity
Modern Handling Equip-
ment
Private Siding on C&NW
CMS&P, G&W Lines
Reciprocal Switching all
lines

Complete local and over-the-road truck services
with 70 units of all types of equipment, including
low-bed trailers, winches and cranes.

AERO-MAYFLOWER MOVING AND STORAGE

MADISON, WIS.

2302 Darwin Road
Phone Cherry 4-6255

HANSEN STORAGE

OF MADISON, INC.

MODERN ONE STORY OPERATION

MILWAUKEE, WIS.

Private Phone: Broadway 1-8930

AMERICAN WAREHOUSE CO.



General Office—525 E. Chicago St.
3 Clean, Well-Equipped Warehouses
Pool Car/Truck Distribution Specialists
Customer Preference—Our Best Reference
Private Siding: C & NW Ry.
3rd Ward Dist.

MILWAUKEE, WIS.

Flagstone 1-6673

BAY-NEL TERMINAL

3119 W. Mill Rd. Milwaukee 9, Wisc.

Warehousing — Distribution — Private Rail Siding
Modern One Story Building

HANSEN STORAGE CO.

124 N. JEFFERSON ST.
MILWAUKEE, WIS.
WISCONSIN'S
LARGEST AND
MOST MODERN

AFFILIATE — HANSEN STORAGE OF MADISON INC.

MILWAUKEE, WIS.

—Phone Marquette 8-7091

TERMINAL STORAGE CO.

100-112 W. Seeboth St.
Milwaukee 4, Wisconsin

Cooler, Freezer and General Merchandising Storage
Deep Water Dock, Private Siding
on C.M.St.P. & P.R.R.

National Warehouse Corp.

MILWAUKEE'S FINEST



A SOLID BLOCK OF
RESPONSIBLE WAREHOUSING

ALSO LEASE RENTALS OF
WHOLE BLDGS. OR PARTS
SO. WATER & E. BRUCE STREETS

SHAWANO, WIS.

Modern Building, Reinforced Concrete Construction

SHAWANO TERMINAL WAREHOUSE

120 E. Richmond Street Shawano, Wisconsin

General Merchandise Storage
LOW INSURANCE RATES
POOL CAR DISTRIBUTION

Licensed and Bonded. Private Siding Chicago & Northwestern R.R.
Member Wis. W.A.

CANADA

TORONTO, ONT.

Established 1913

TORONTO'S LARGEST

Merchandise Storage and Distribution Specialists



HOWELL WAREHOUSES LIMITED

Head Office: 222 FRONT ST. EAST TORONTO 2, CANADA

Telephone: EMpire 4-0111

Member of Canadian Warehousemen's Association

TORONTO, ONTARIO TERMINAL WAREHOUSES LIMITED

1,000,000 Square Feet

Merchandise Storage

Refrigerated Storage Field Warehousing
Office Space Cartage
Stevedoring
Rail Siding
Fireproof

Free Switching
Lowest Insurance

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ENQUIRIES INVITED

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(Established 1916)

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General Merchandise—Bonded & Free

1,500,000 cubic feet. Sprinklered.

Private Siding. Efficient Loading Facilities.

Company Owned Trucks.

Members of Canadian Warehousemen's Association.

"Our Clients do the selling—we do the rest."

MONTREAL, QUE.

St. Lawrence Warehouse Inc.

1-VAN HORNE AVENUE, MONTREAL, CANADA

200,000 SQ. FT. OF MODERN FIREPROOF SPACE LOCATED

IN THE EXACT CENTER OF THE CITY OF MONTREAL

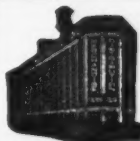
Canadian Customs Bonded. Private Siding — 8 Car

Capacity — Free Switching — All Railroad Connections.

Represented by

ALLIED DISTRIBUTION, INC.

CHICAGO, ILL. NEW YORK, N. Y.



All Delta Flights Carry airFREIGHT



**Serving 60 Cities in
7 Countries**

Leading companies in nearly every field have learned how to improve distribution, build sales and cut costs with Delta airFREIGHT. Find out what flying freight can do for you, how to "air" your shipping problems. For answers to specific questions—or complete shipping analysis, free—call your local representative of Delta airFREIGHT. Or write to:



airFREIGHT Dept., Atlanta Airport,
Atlanta, Georgia

CLASSIFIED ADVERTISING

RATES: 20¢ a word—\$5 minimum

SALE OR LEASE

FOR RENT—Excellent storage warehouse, Bristol, Pa. 12 to 85,000 feet, 10 acres of land. One story, high ceilings, heated, sprinklered, traveling cranes, PRR sidings, Del. Valley Interchange Turnpike. Write Box 221, DISTRIBUTION AGE, Chestnut & 56th Sts., Philadelphia 39, Pa.

TRAILERS FOR SALE—Asphalt, Acid, and Petroleum trailers of various sizes. Contact Gingerich at 1026 So. Riverside Drive, Iowa City, Iowa. Phone 8-3691 or 4719.

DO YOU HAVE A WAREHOUSE OR BUILDING THAT NEEDS HEAT?—For Sale: Six almost new 2,000,000 BTU/hr. Model HD-2000 Lee oil fired industrial space heaters. Manufactured new in 1952 and used for one season only. Complete with all controls and 15 hp 3/60/220-440 blower motors. Price FOB Chicago \$1600 each. ESTES MACHINERY CO., 821 W. Lake St., Tel.: Monroe 6-1814, Chicago 7, Ill.

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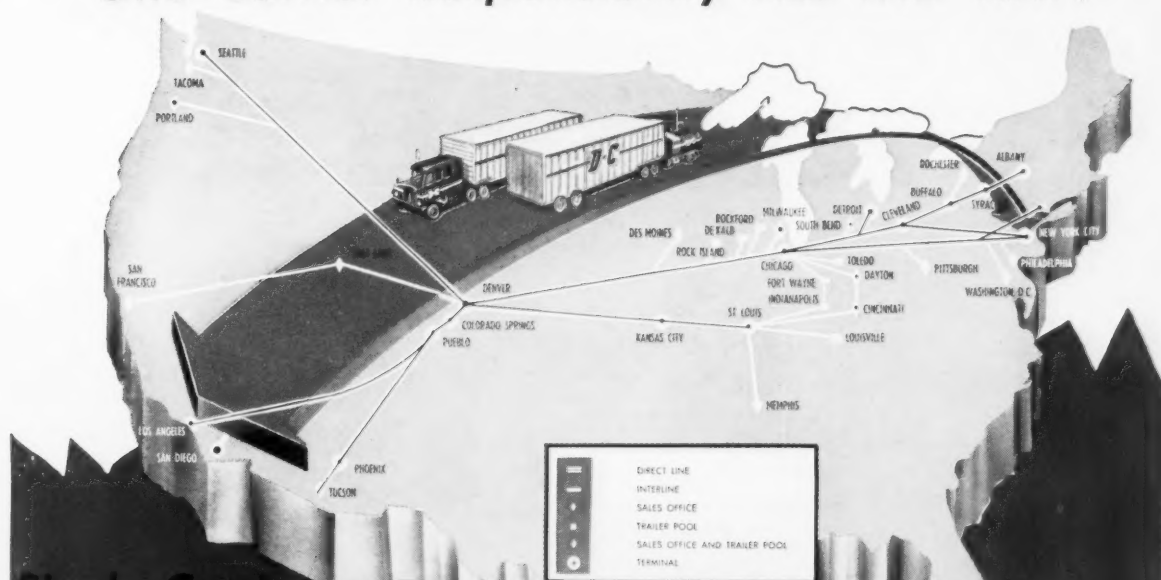
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D-C Goes Straight Through **COAST-TO-COAST**

with NON-STOP, 2-MAN SLEEPER CABS
One Carrier Responsibility ALL THE WAY!



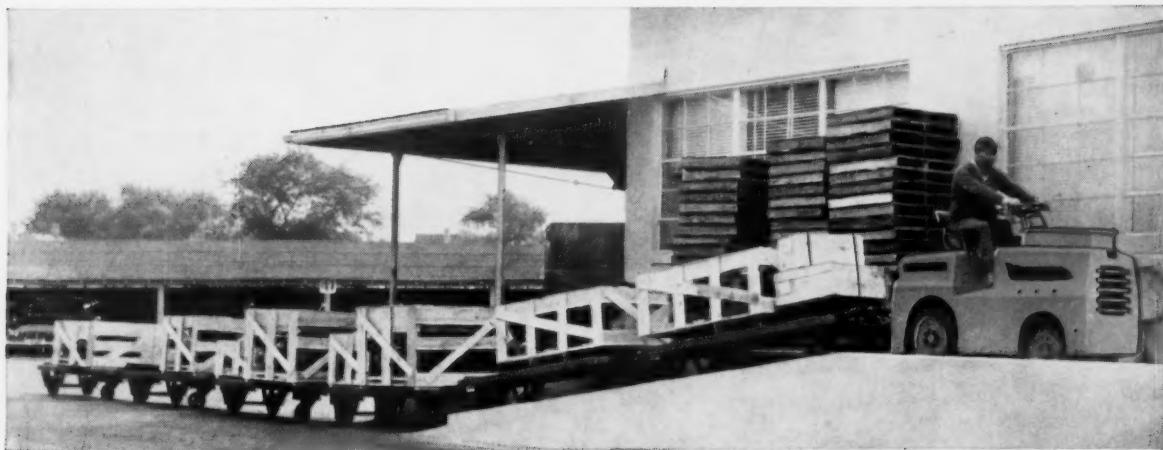
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- 1. Handling**
- 2. Tracing**
- 3. Insurance**
- 4. Safety**
- 5. Delivery Information**

THE ONLY COAST-TO-COAST CARRIER



Denver Chicago **TRUCKING CO., INC.**



DYNAMOTIVE Tractor above "walks away" with 19,225 lb. load up 12% grade at Robbins & Myers plant.

**NOW...A GAS TRUCK THAT DOES
MORE WORK ON 25% TO 30% LESS GAS...**

Automatic DYNAMOTIVE

The secret of Automatic DYNAMOTIVE'S (GLF Series) proved superior performance lies in its utter simplicity...an engine-driven variable voltage generator supplies electrical power to the drive motor direct coupled to the drive axle. This proved power train *without gears* eliminates slippage and power loss through heat build up. Additional gas mileage is achieved by the inherent overdrive and torque multiplying features of the electric power train.

Among many other advantages are the following:

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Tilt and Lift Controls...optional for either foot or fingertip operation.

Full Width Posture Seat...accessible from either side...reduces driver fatigue.

Visibility...high visibility of fork position throughout lifting range.

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G.L.F. Capacities: 4,000 to 10,000 lbs. with solid tires...4,000 and 6,000 lbs. with pneumatics. Tractors available in 2,500 and 3,500 lbs. draw bar pull.

But see for yourself. *Ask for demonstration* and/or complete specifications with case histories of installations similar to your own. No obligation...write today.



Part of a **DYNAMOTIVE "fleet"** used for warehousing, loading, etc. Calumet Industrial District Co.



This **DYNAMOTIVE** unit paid for itself in one year says Lock Joint Pipe Company official.

Factory service available in over 100 cities.

Automatic

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